

NORTHERN JUNK REZONING/HERITAGE ALTERATION PERMIT

June 19 2019

ADDRESS

1314 Wharf Street, Victoria, BC

OWNER

**CROSSTOWN PROPERTIES,
MANAGED BY RELIANCE PROPERTIES**

111 Water St #305, Vancouver, BC
(604) 683-2404

ARCHITECT

DIALOG

611 Alexander St #411, Vancouver, BC
(604) 255-1169

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APPENDIX (BOUND SEPARATELY)

1 ADDITIONAL INFORMATION
FULL SIZE ARCHITECTURAL DRAWINGS
FULL SIZE LANDSCAPE DRAWINGS
TRAFFIC MEMO
HERITAGE MEMO
HERITAGE CONSERVATION PLANS

01

PROJECT INTRODUCTION



AREA SUMMARY

ASSSUMPTIONS		
	SQ.FT	m2
SITE AREA (ABOVE PRESENT NATURAL BOUNDARY)	13,107	1218
MAX ALLOWABLE DENSITY (4.4)	57,672	5358
EXISTING FOOTPRINTS	5,175	481

AREAS BY USE		
CRU 2 - L01	231.9 sm	
CRU 1 - L01	228.4 sm	
CRU 1 - L00	212.7 sm	
CRU 2 - L00	200.4 sm	
TOTAL COMMERCIAL	873.4 sm	(9,401.3 sf)
Building Gross Area	4136.2 sm	
Total Commercial Area	873.4 sm	(9,401 sf)
TOTAL RESIDENTIAL	3262.8 sm	(35,120.8 sf)
NET RESIDENTIAL	2649.4 sm	
NET RENTABLE (RES+COMM)	3,523 sm	

SITE AREAS (SEE AREA PLANS A-010)	
GROSS SITE AREA	1376 sm
NET SITE AREA	1218 sm
DFW SRW AREA	199 sm
OPEN AREA	453 sm
BUILDING FOOTPRINT	765 sm
OPEN AREA %	37.2%
NET SITE AREA - DFW S.R.W. AREA	1018.5 sm

BUILDING GROSS AREA	
L00 Gross	398 sm
L00 Gross	213 sm
L00 TOTAL	611 sm
L01 Gross	251 sm
L01 Gross	321 sm
L01 TOTAL	572 sm
L02 Gross	738 sm
L02 TOTAL	738 sm
L03 Gross	738 sm
L03 TOTAL	738 sm
L04 Gross	738 sm
L04 Total	738 sm
L05 Gross	738 sm
L05 Total	738 sm
BUILDING GROSS	4,136 sm
SITE AREA FOR FSR	1,218 sm
FSR	3.40
NET RENTABLE (RES+COMM)	3,523 sm

UNIT SUMMARY

UNIT TYPES						
LEVEL	STUDIO	1BD	2BD	3BD	-	TOTAL UNITS
LEVEL P1	0	0	0	0	0	0
LEVEL 01	0	0	0	0	0	0
LEVEL 02	1	8	2	1	0	12
LEVEL 03	1	8	2	1	0	12
LEVEL 04	1	8	2	1	0	12
LEVEL 05	1	6	3	1	0	11
LEVEL 06	0	0	0	0	0	0
TOTAL	4	30	9	4	0	47
PERCENTAGE	9%	64%	19%	9%	0%	100%
FAMILY UNITS	28%					

AVERAGE UNIT SIZES					
LEVEL	TOTAL AREA PER FLOOR BY TYPE				
	STUDIO	1BD	2BD	3BD	TOTAL UNITS
LEVEL 00	0	0	0	0	0
LEVEL 01	0	0	0	0	0
LEVEL 02	37	392	151	82	662
LEVEL 03	37	392	151	82	662
LEVEL 04	37	392	151	82	662
LEVEL 05	37	286	257	82	662
TOTAL	150 sm	1,461 sm	709 sm	329 sm	2,649 sm
Unit Count	4	30	9	4	47
AVG SIZES	37 sm	49 sm	79 sm	82 sm	56 sm
AVG SIZES (SF)	403.3 sf	524.4 sf	848.5 sf	884.2 sf	606.8 sf

PARKING SUMMARY

NOTE: NO VEHICLE PARKING PROVIDED ON SITE. SEE TRAFFIC REPORT FOR TRAFFIC MANAGEMENT STRATEGY

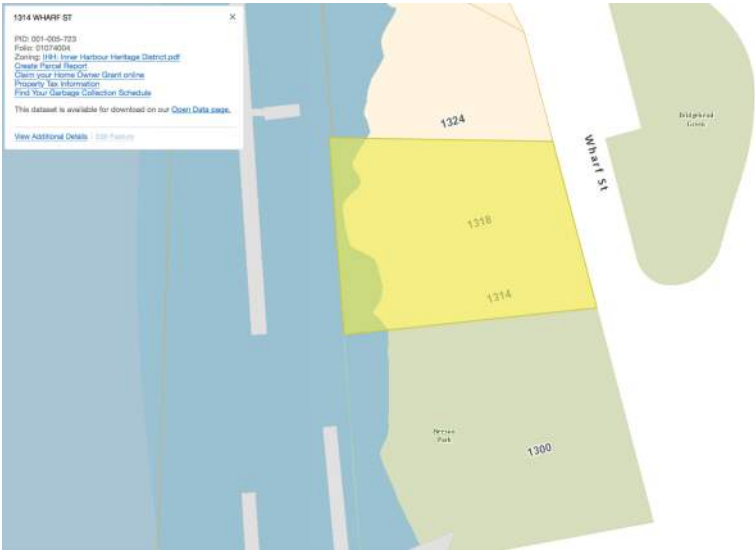
BICYCLE PARKING			
Residential	Count	Req'mt	Provided
1.0/ Unit < 45m2	12	12	
1.25/ Unit > 45m2	35	43.75	
Total Residential	47	55.75	66
Commercial	Area	Req'mt	Provided
1.0/ 200 m2	873.4m2	4.4	
Total		5	5

PROJECT DATA		
SITE AREA		
NET AREA (ABOVE NATURAL BOUNDARY)		1218 m²
GROSS AREA		1376 m²
TOTAL FLOOR AREA		4140 m²
HEIGHT		
FROM AVERAGE GRADE		20.5 m
FROM STREET GRADE		18.8 m
NUMBER OF STOREYS		5 storeys from Wharf St. 6 storeys from Harbour
DENSITY		
MAX ALLOWABLE DENSITY		4.0
FSR		3.40
BUILDING AREAS		
COMMERCIAL		875 m²
RESIDENTIAL		3,265 m²
OPEN AREA %		37.2%
TOTAL		4,140 m²
UNIT TYPES		
STUDIO		9%
1 BED		64%
2 BED		19%
3BED		9%
FAMILY UNITS		28%
TOTAL UNITS		47
UNIT SIZES (AVERAGE)		
STUDIO		37 m²
1 BED		49 m²
2 BED		79 m²
3 BED		82 m²
BICYCLE PARKING		
RESIDENTIAL		66 stalls
COMMERCIAL		5 stalls
TOTAL		71 stalls
SETBACKS		
FRONT YARD (EAST)		0 m
REAR YARD (WEST)		13.84 m
SIDE YARD (NORTH)		0 m
SIDE YARD (SOUTH)		0 m

APPLICABLE POLICY REVIEW:

- ZONING:
- OFFICIAL COMMUNITY PLAN (OCP)
- DOWNTOWN COMMUNITY AREA PLAN (DCAP)
- SCHEDULE C PARKING
- OLD TOWN GUIDELINES
- CITY OF VICTORIA HOUSING STRATEGIES
- GREEN BUILDING INDICATORS

- **ZONING:** INNER HARBOUR HERITAGE DISTRICT (IHHD)



CURRENT ZONING

PROPOSED VARIANCES

		8.22
	PART 8.22 – IHH ZONE – INNER HARBOUR HERITAGE DISTRICT	
Permitted Uses	1	The following uses are permitted: (a) retail shops (b) <u>clubs</u> (c) <u>restaurants</u> (d) licensed premises; (e) artists' studios; (f) docks for pleasure boats and fishing boats; (g) recreation facilities; (h) beauty and barber shops; (i) residential use, when the use is located at least one floor above the grade at Wharf street; (j) craft and artisan trades, not offensive, dirty or noisy; (k) parks.
Density	2	The <u>floor space ratio</u> may not exceed 1.0 to 1.
Height	3	No <u>building</u> may extend more than 8m above Wharf Street.
Setback	4	The minimum <u>setback</u> from the high water mark shall be 7.5m.
	5	No off-street parking or loading spaces are required.

REZONE TO ALLOW FOR 4.0 FSR

REZONE TO ALLOW FOR 5 STOREYS WITHIN OCP

- **OFFICIAL COMMUNITY PLAN:** January 17 2019

Note: No OCP amendments sought

Urban Place Designation: Core Inner Harbour Legislative

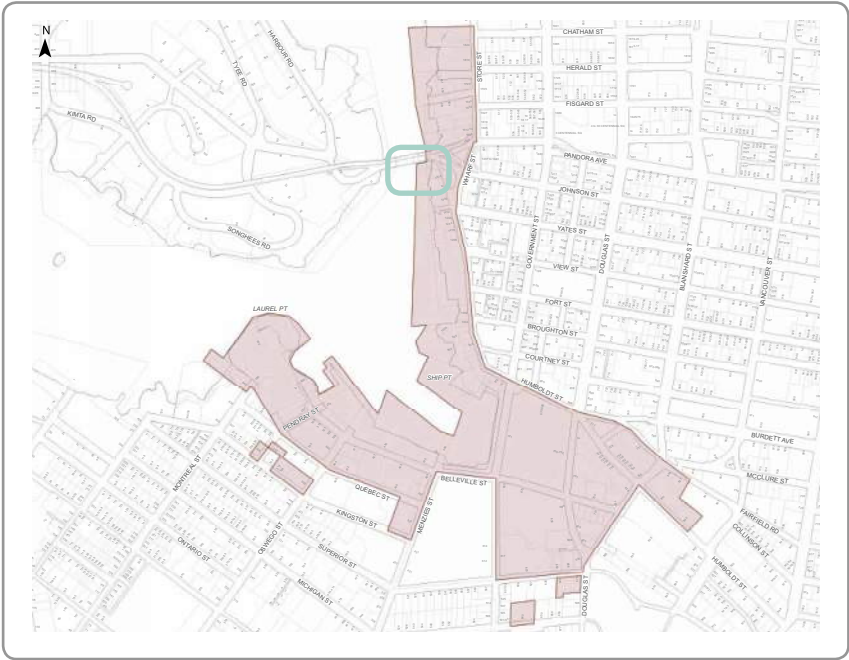
- 1-5 storeys
- Up to 4:1 FSR
- Multi-unit residential; Commercial



Urban Place Guidelines: Core Inner Harbour Legislative

Designation	Built Form	Place Character Features	Uses	Density
Core Inner Harbour/ Legislative	<div>Buildings from one to five storeys adjacent to the harbour.</div> <div>Buildings up to approximately five storeys south of Superior Street and along Menzies Street and Government Street facing the Provincial Parliament Buildings.</div> <div>Buildings up to approximately 15 storeys east of the Empress Hotel to Blanshard Street, and south of Belleville Street.</div>	<div>Formal arrangement of buildings and their grounds deployed to respect the form and character of signature landmarks, including the Empress Hotel, and the Provincial Parliament Buildings.</div> <div>Formal public realm comprising wide sidewalks, pedestrian promenades, regularly spaced tree planting, formal squares and greens, pathways along the harbour, and prominently placed public art.</div> <div>Active marine transportation including ferry staging and moorage.</div> <div>Site of major festivals, celebrations and special events.</div>	<div>Public institutional and assembly.</div> <div>Commercial, including office, retail, and visitor accommodation.</div> <div>Marine water and air transportation.</div> <div>Recreation and tourism-related uses.</div> <div>Multi-unit residential and mixed-use.</div> <div>Home occupations.</div>	<div>Total floor space ratio generally ranging up to 1:1. Increased density up to a total of 4:1 may be considered in strategic locations for the advancement of plan objectives.</div>

Development Permit Area: DPA 9 (HC):Inner Harbour



Project addresses several OCP plan objectives such as:

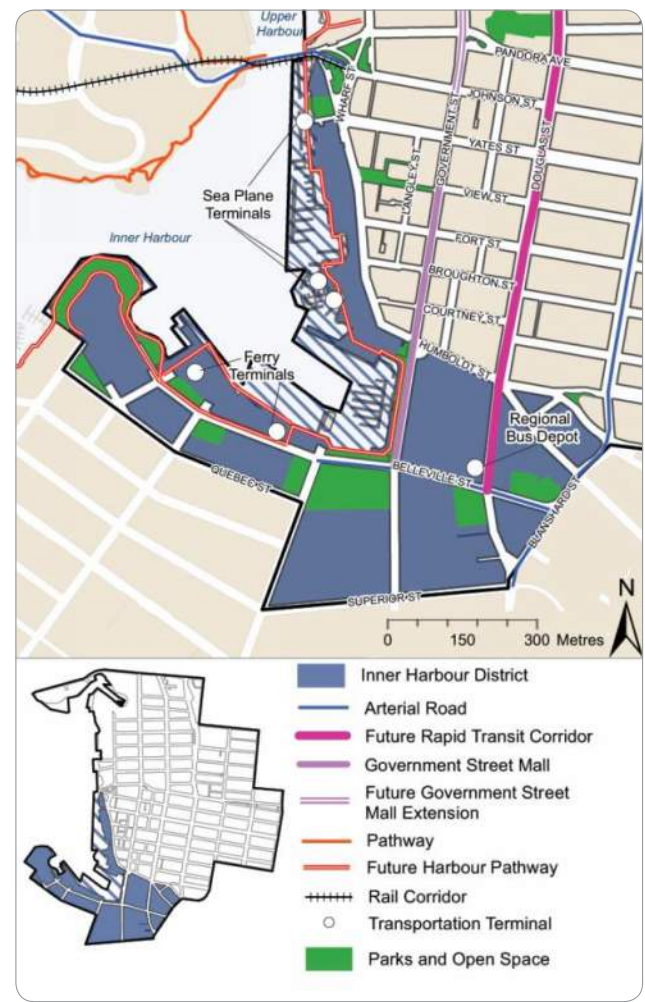
- Housing diversity
- Public amenity (Harbour Pathway)
- Upgrade of Heritage buildings
- Support economic activity in harbour
- Vitality and Livability
- Responsive to Victoria’s geographic context and existing pattern of development, achieves excellence, and creates memorable places.
- Contributes to the sense of place through sensitive and innovative responses to existing form and character.
- Social vibrancy is fostered and strengthened through human scale design of buildings, streetscapes and public spaces.
- That the built environment is beautified and softened through natural features in the public realm.
- Heritage property is conserved as resources with value for present and future generations.
- That streetscape improvements include art in public places and reflect the culture and heritage of Victoria.
- Consistent with OCP Policy and RGS Strategic Direction for Compact Urban Settlements.
- Supporting extension of multi use pathway along waterfront with a priority for pedestrian and cycling transportation modes
- Streetscape improvements include art in public places and reflect the culture and heritage of Victoria



• DOWNTOWN CORE AREA PLAN: INNER HARBOUR DISTRICT

Project addresses several DCAP objectives such as:

- To successfully maintain and strengthen the IHD as the focus for tourism, government, culture, heritage, and economic development.
- To develop and maintain a cohesive, well-designed and vibrant waterfront area.
- To create a more fluid and seamless extension of the public realm northward toward the Johnson Street bridge and beyond, toward the Rock Bay District.
- To improve public access to the waterfront.
- To maintain a working Harbour.
- Increased Housing options
- Support seismic upgrading of heritage buildings
- Improving vitality and livability of downtown core area



• PARKING: SCHEDULE C

Looking to standard from adjacent OTD-1 zone:
Motor vehicle parking is only required for any Lot that has an overall area of 1100m2 or greater, and subject to paragraph (b), shall be provided in accordance with Part 5 of this bylaw.

Taking into account the SRW provided for the waterfront walkway, the remaining developable site area falls below the 1100m2 threshold for parking requirements. The presence of the two heritage structures further compounds the challenge of providing on-site parking. Presented with the choice of preservation vs. demolition for parking, the proposal is to eliminate provision of parking. All bicycle parking requirements are satisfied on site. No change requested relative to existing IHH zoning.

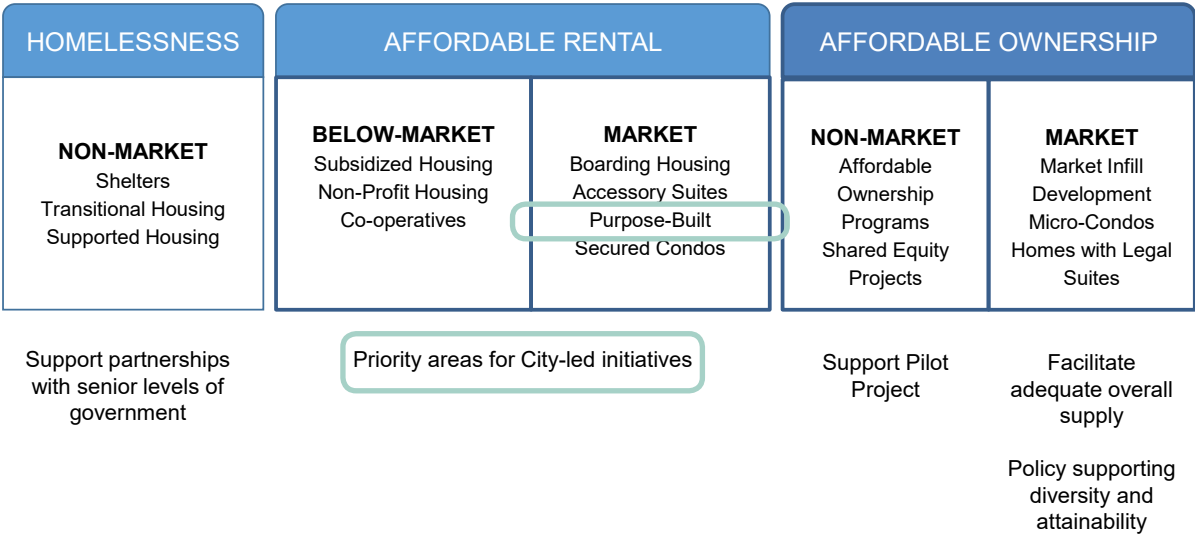
See additional description and rationale provided in Bunt Traffic report included as an appendix to this document



• CITY OF VICTORIA HOUSING STRATEGY: 2016-2025

Project addresses several Housing Strategy objectives such as:

- Increase supply of purpose built rental housing to meet current and future needs
- Long term stable rental supply



• GREEN BUILDING INDICATORS

CATEGORY	GREEN ITEMS
RATING SYSTEM	N/A
SITE SELECTION	N/A
INNOVATION AND DESIGN	-DESIGN TEAM WILL BE PRODUCING AN ENERGY MODEL TO OPTIMIZE BUILDING ENERGY PERFORMANCE
BUILDING RETENTION AND REUSE	-2 HERITAGE BUILDINGS BEING RETAINED AND RESTORED ON SITE
TRANSPORTATION	-NO PARKING PROVIDED ON SITE - CONSIDERATION OF CAR SHARE PROGRAM PARTNERSHIP
ENERGY EFFICIENCY (PART 3)	- DESIGNED TO COMPLY WITH STEP 3 OF THE BC ENERGY STEP CODE - ENERGY MODEL WILL USED IN DETERMINED BUILDING MECHANICAL SYSTEMS
RENEWABLE ENERGY (PART 3)	N/A
WATER	- ALL PLUMBING FIXTURES TO MEET OR EXCEED PLUMBING CODE FLOW RATES - HIGH EFFICIENCY APPLIANCES (I.E. DISHWASHER, CLOTHES WASHERS)
SITE PERMEABILITY	PRIMARILY PERMEABLE PAVING ON SITE
LANDSCAPING AND URBAN FOREST	NO NET LOSS IN NUMBER OF TREES ONLY NATIVE VEGETATION PLANTED
URBAN AGRICULTURE	N/A



02

LETTER TO THE MAYOR

INTRODUCTION:

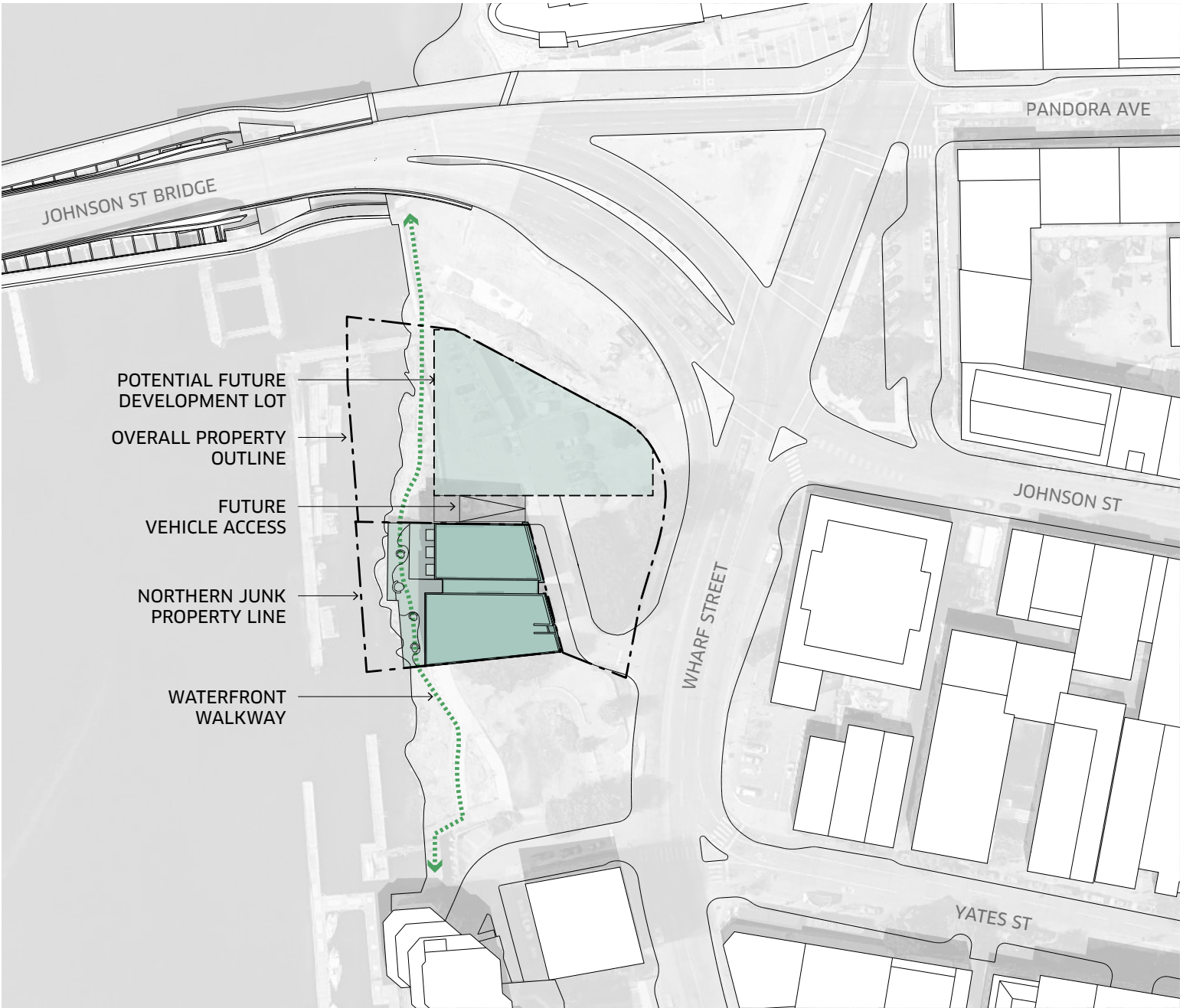
To Mayor & Council,

The Northern Junk site at the head of the Johnson Street Bridge presents unique set of opportunities and challenges. The site is in a prominent position in the City between Old-Town and Inner-harbour. Currently isolated between Reeson Park to the south and the as-yet undefined Bridgehead Green park to the North, the site offers the potential to complete a key piece of the public realm and neighbourhood revitalization through its design and development.

The proposed development responds to a number of planning and urban design objectives set forth in the OCP and Core Area Plans. In particular, it will provide housing diversity in the form of new rental apartments, an upgrade of two heritage buildings, improve public access to the waterfront and support economic and social vitality in the area, all within a built-form that is in keeping with the goals of area design guidelines.

The project consists of a multi-unit residential apartment building set atop the existing Northern Junk warehouse buildings (Caire & Grancini Warehouse and Fraser Warehouse) that are to be rehabilitated and incorporated into a mixed-use development. The project brings together active ground level uses, new housing options, and an integrated public access and extension of the public waterfront walkway.

Previous iterations of the project envisioned a comprehensive redevelopment of City-owned lands to the East and North of the site, the current proposal seeks -within a much smaller footprint- to still deliver public realm and heritage preservation and rehabilitation, and contribute to the economic, social, and architectural vitality of the neighbourhood. The nature of the development however becomes less about a landmark structure marking a gateway, but rather an extension of built patterns in the area leaving the City-owned lands with potential for future development.



DESIGN RATIONALE REVIEW:

The design of the project has three primary drivers: the existing heritage warehouse buildings, response to public realm and response to the patterns and character of Harbour and Old-town.

- HERITAGE RESPONSE
- PUBLIC REALM RESPONSE
- ARCHITECTURE, MASSING & MATERIALS

HERITAGE RESPONSE:

(See also conservation plans & heritage consultant’s rationale)

The existing architecture of the two Northern Junk warehouse buildings is to be rehabilitated by careful revealing of original facade elements now obscured by previous alterations and additions and rehabilitating the exterior masonry facade. Alterations including new openings are to be added to improve the functionality of the buildings. The key goal has been to maintain as many elements of the existing buildings as possible while giving them a new life as active commercial spaces accessible to the public. As much as possible, the new elements of the project have been devised to maximize visibility and retention of the existing buildings and bring new value to them for present and future generations. The new structure and spaces are fused with or enveloping of the heritage elements allowing users to come in contact with and be able to appreciate the heritage elements in new ways. All principal facades, interior masonry walls and openings are retained in the proposed design.

Whereas, a typical addition to a heritage building might be smaller in scale, the context of the buildings within the generally 5-storey Old-town fabric suggests that a one or two-storey addition would seem out of scale with the surroundings. The increased height of the additions delivers an integrated streetscape using the heritage buildings as its foundation. The resulting tripartite composition of each facade reinforces the classical pattern of base (heritage buildings), middle (new residential building), and top (contemporary cornice elements).

Glass treatment enclosing the south and west facades of the Caire & Grancini building offers another view of building preservation as part of the overall conservation of the two buildings, and enables the heritage building to become, in part, an artifact within a controlled environment.

The patterns of glazing and masonry piers of the new elements above the two heritage buildings are guided by the existing proportions and details of their facades, rendered in a simpler more contemporary articulation so as to not compete with the existing facades, but be sympathetic and complementary.

Design responds to the new Draft Old Town Design Guidelines as follows:

- Locate and site new buildings and additions to create a continuous ‘street wall edge’.
- Design new buildings and additions to reflect the established proportions, composition and spatial organization of adjacent historic facades.
- Include a distinct roof line in the building design, through such measures as a simplified or contemporary cornice or parapet.
- Modulate the overall horizontal and/or vertical building mass on large buildings to achieve a scale that is compatible with adjacent buildings.
- For buildings over 30m in width, incorporate an articulated vertical break in the facade with a patio, courtyard, recessed entryway or other features to help break up the expanse of the facade.
- Avoid buildings with a dominant horizontal expression in favour of more compact buildings that reflect the smaller scale vertical expression of existing heritage buildings.
- Locate and design new buildings along the waterfront to provide direct pedestrian access to the Harbour Pathway where appropriate.
- Design new rooftop additions with contemporary materials and finishes.
- Use a slightly different ratio of solid to transparent materials than the historic building.
- Incorporate setbacks from street-facing elevations to maintain the distinction between old and new construction.

• **OLD TOWN DESIGN GUIDELINES:** INNER HARBOUR DISTRICT

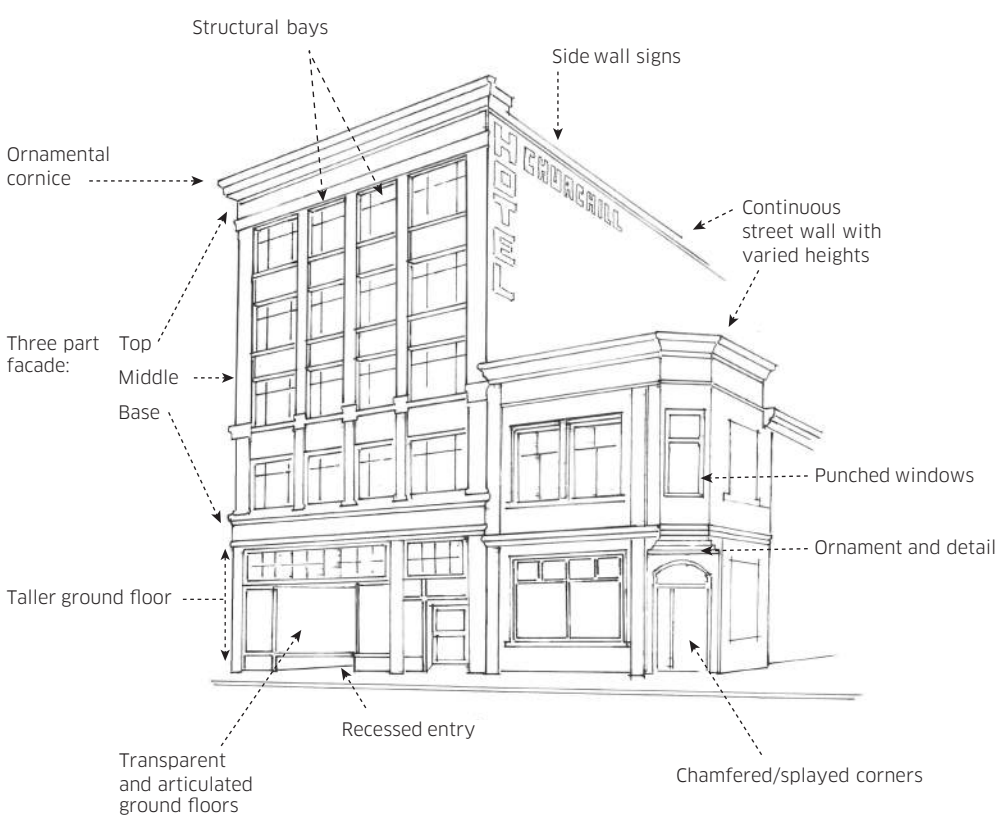
Project addresses several Old Town Guidelines objectives such as:

Character Defining Elements – Old Commercial District

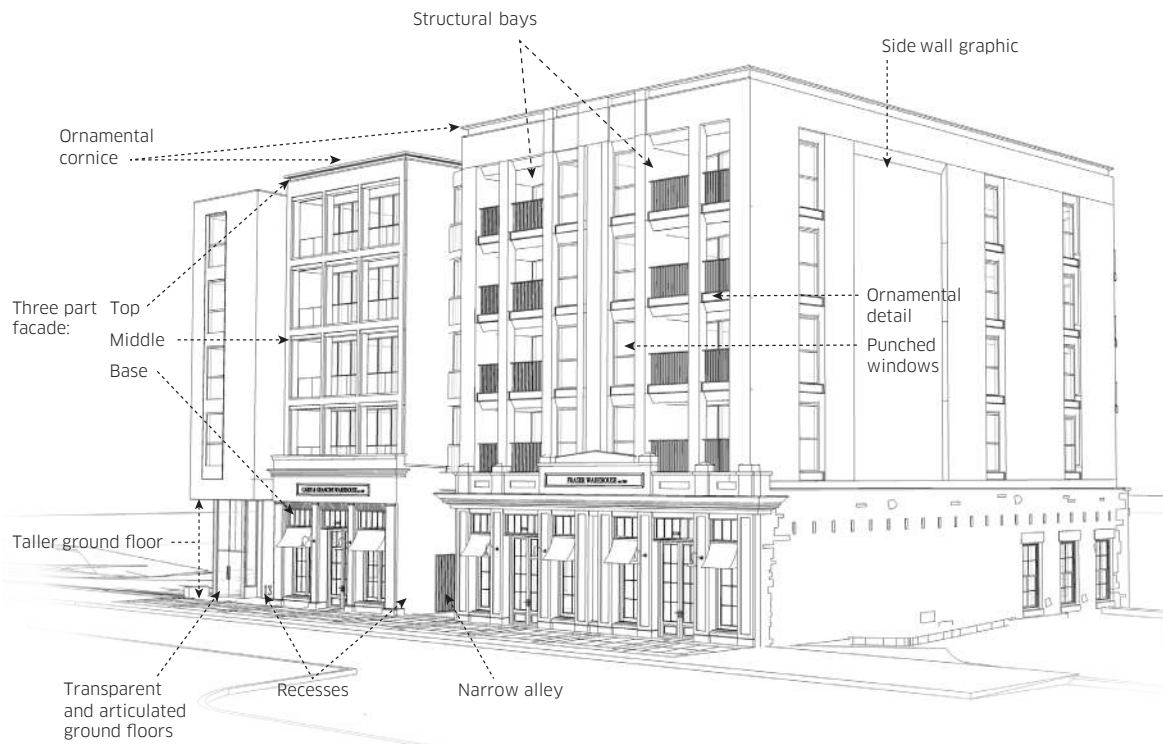
- Classically inspired three part building facades with a clearly defined base, middle and top;
- Vertical facade expressed by use of structural bays, vertical elements and proportions, and punched openings, including upper storey double-hung windows;
- Use of high quality materials such as wood, metal, brick, natural stone and glass;
- Well crafted facade ornamentation and detailing;
- Varied range of low to mid -scale building heights generally ranging from three to five storeys;
- Varied and attractive roof lines along each street that are accented by architectural features such as parapets and cornice lines;
- The prominent use of brick masonry construction
- Prefabricated structural and decorative components of exterior facades of commercial buildings such as wood brackets and tin cornices;
- The presence of pedestrian paths, mews, alleys and courtyards within and through blocks

Character Defining Elements – Waterfront

- Building types and spaces associated with the functions of a commercial harbour;
- Buildings over the water, vessels, float planes, buoys and marks, wharves, pontoons, piles, boat ramps, davits and ladders, hawsers, bollards, and mooring rings;
- Random rubble stonework, brick masonry, and iron shutters and doors;
- Stone and brick retaining walls;
- Buildings with an industrial aesthetic;
- Dual-aspect structures that present a commercial frontage to Wharf Street and a harbour frontage to the water;
- The rich texture of the land/water edge resulting from conditions including inter-tidal beaches, projecting structures, inundations and reclamations;
- Load-bearing masonry buildings and details and forms that accompany load-bearing masonry surrounded by subordinate lighter wood and metal structures;
- The glimpses of water seen between buildings, down alleyways and slips and on street axes.
- The view of Old Town from the water; defined by a concentration of small scale historic buildings tiering up from the waterfront with a distinct rhythm and rich design quality.

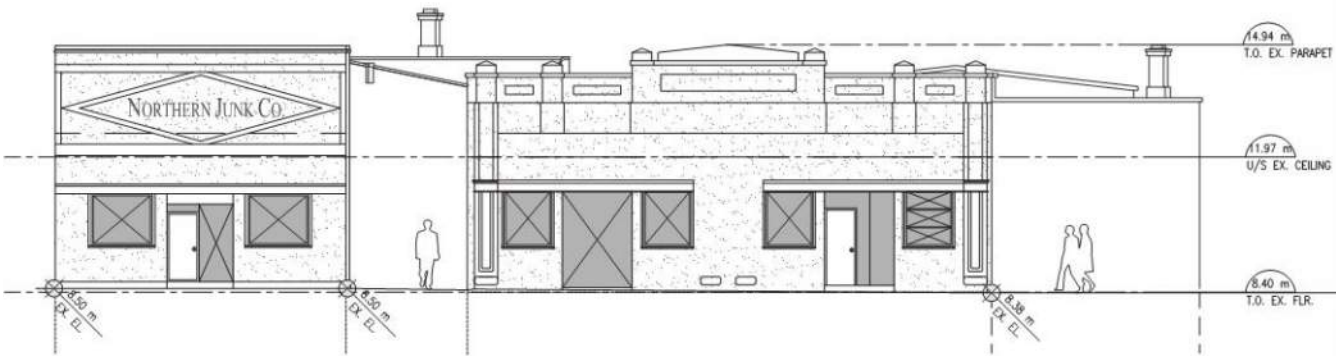
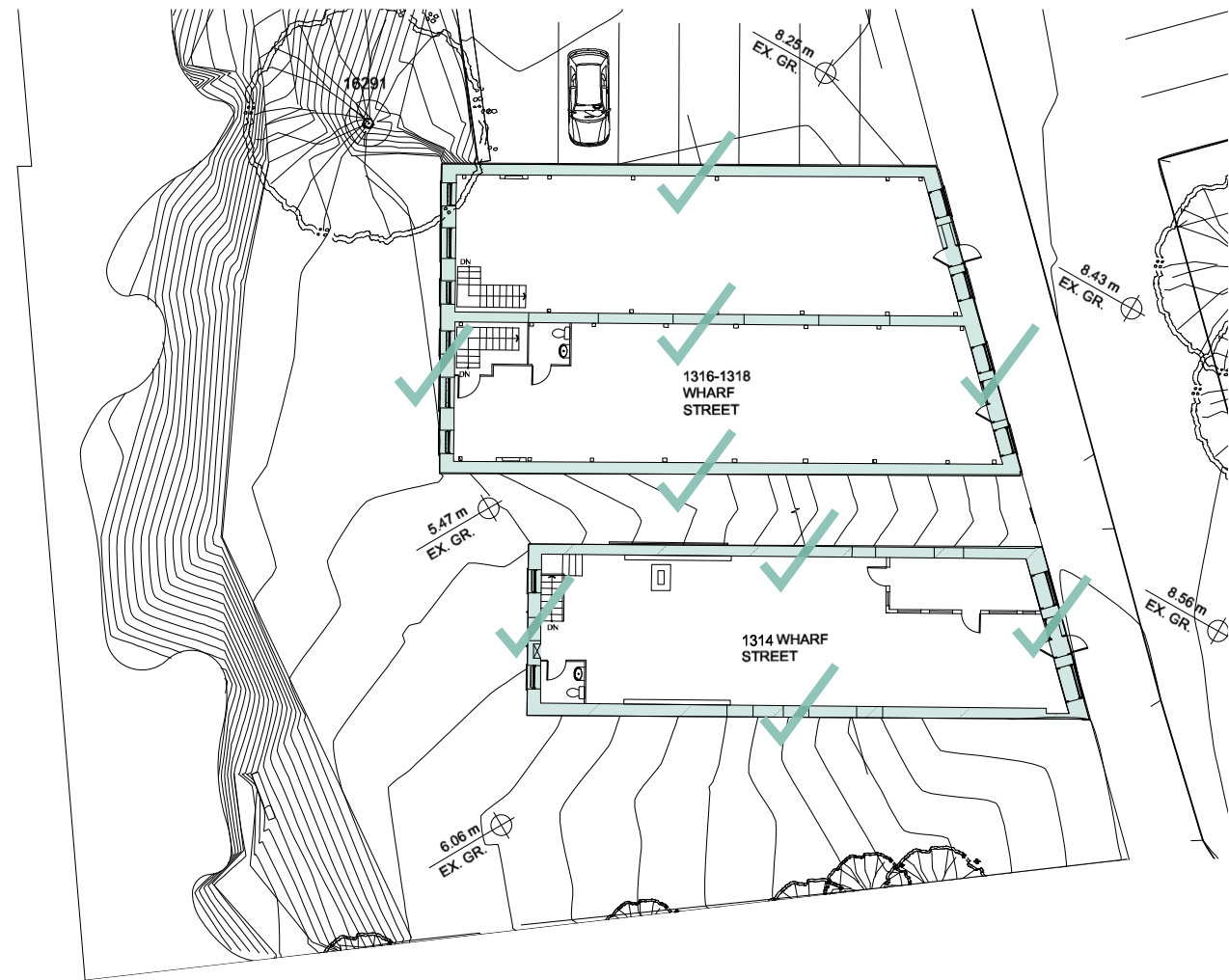


OLD TOWN GUIDELINES

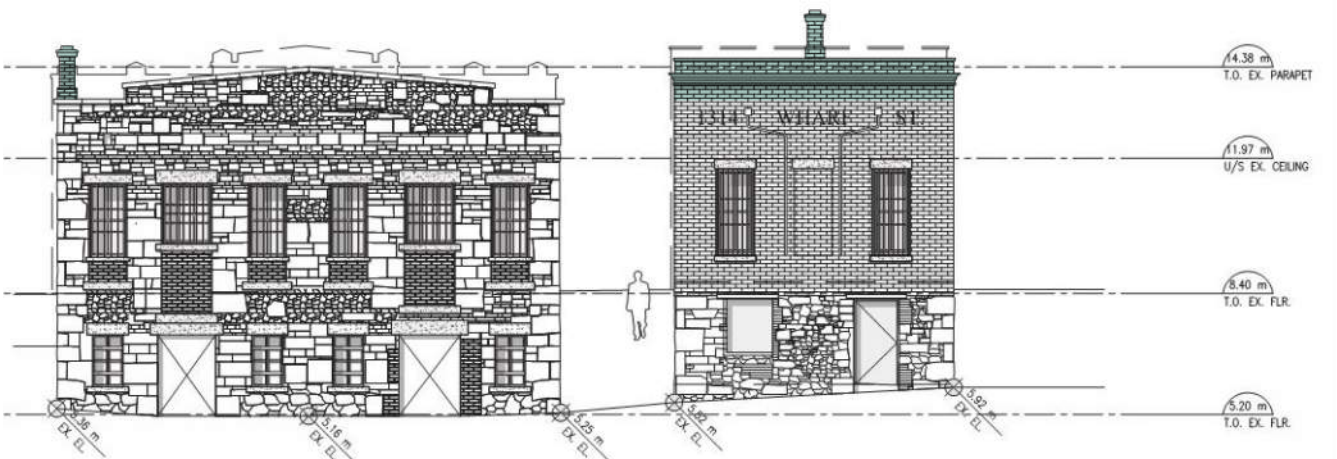


PROPOSED PROJECT

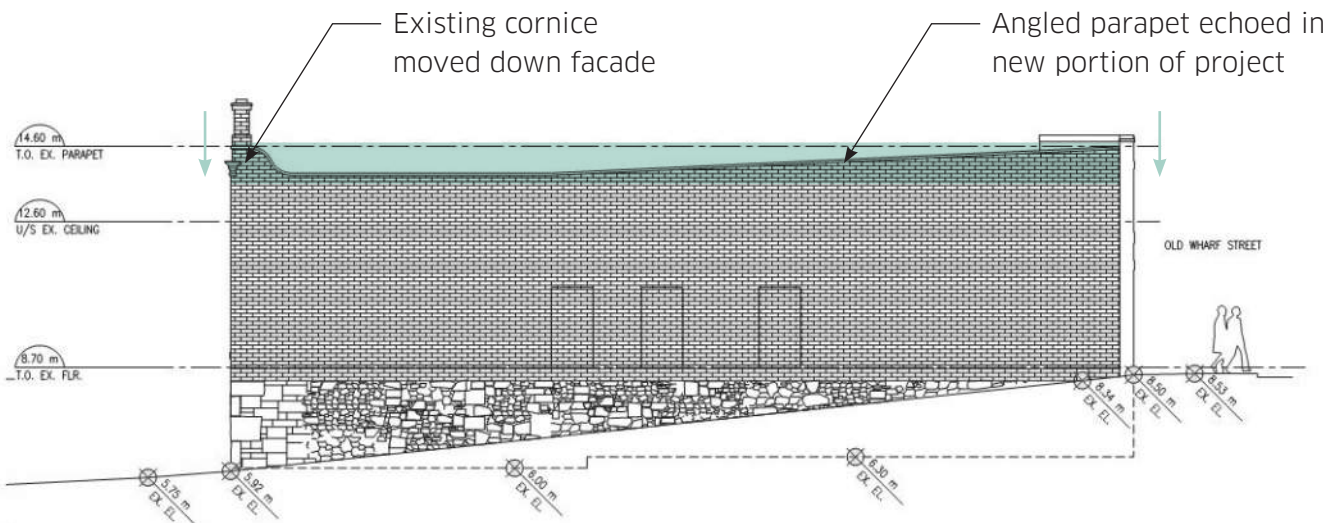
ALL EXTERIOR FACADES RETAINED



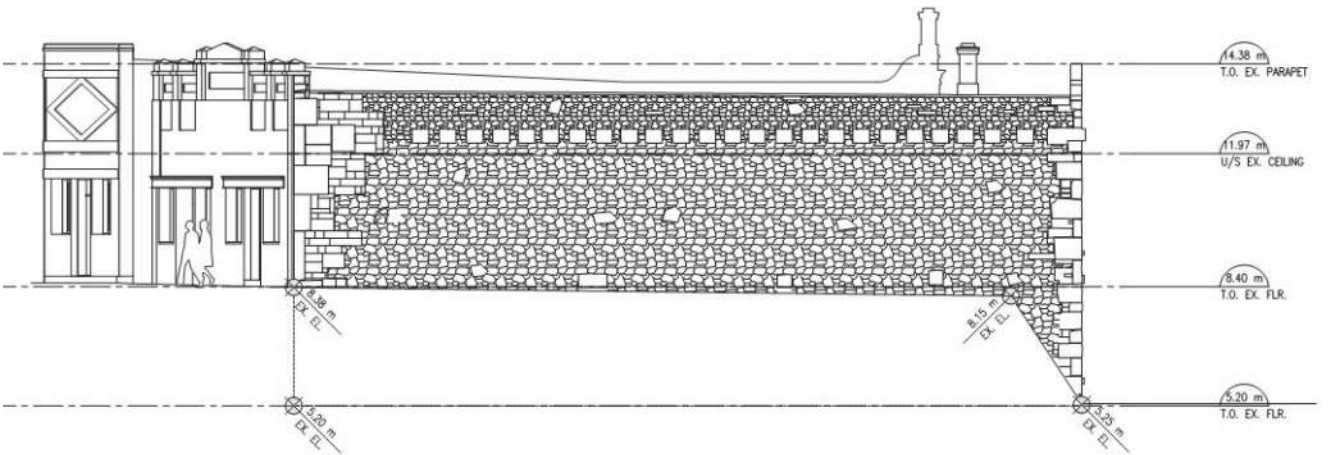
EXISTING EAST ELEVATION



EXISTING WEST ELEVATION



EXISTING SOUTH ELEVATION



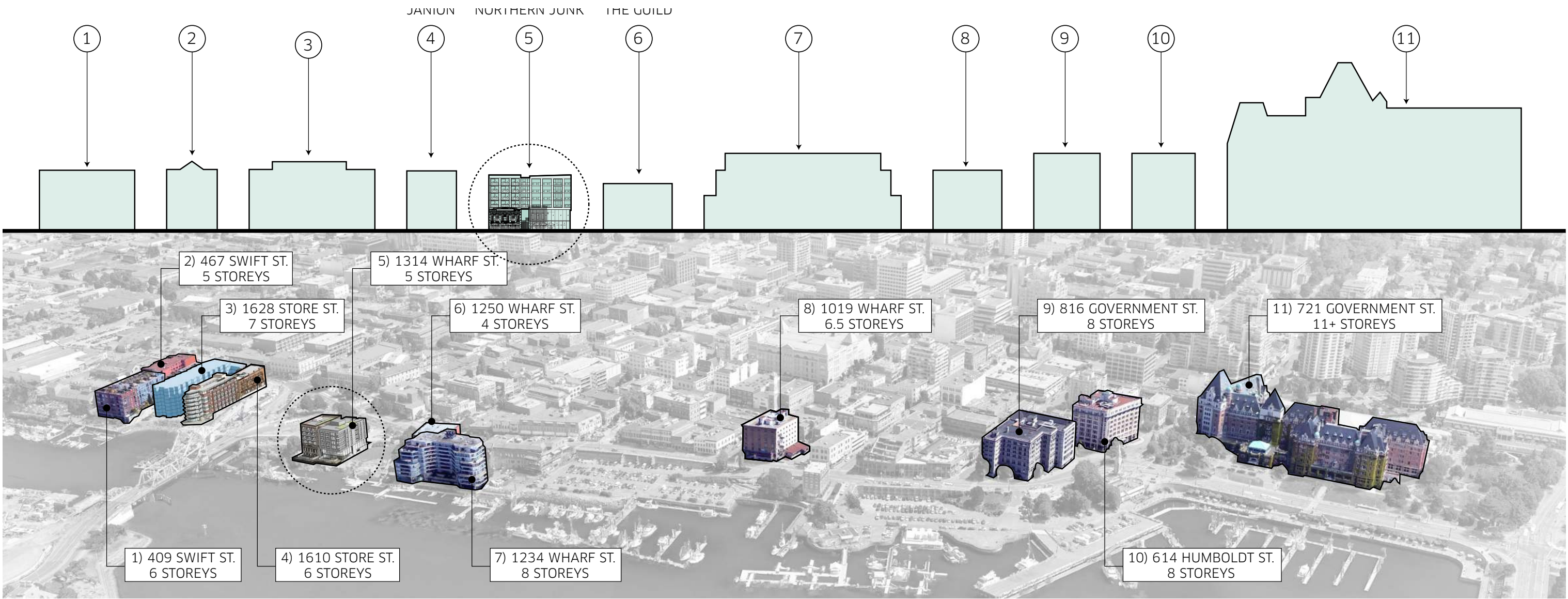
EXISTING NORTH ELEVATION

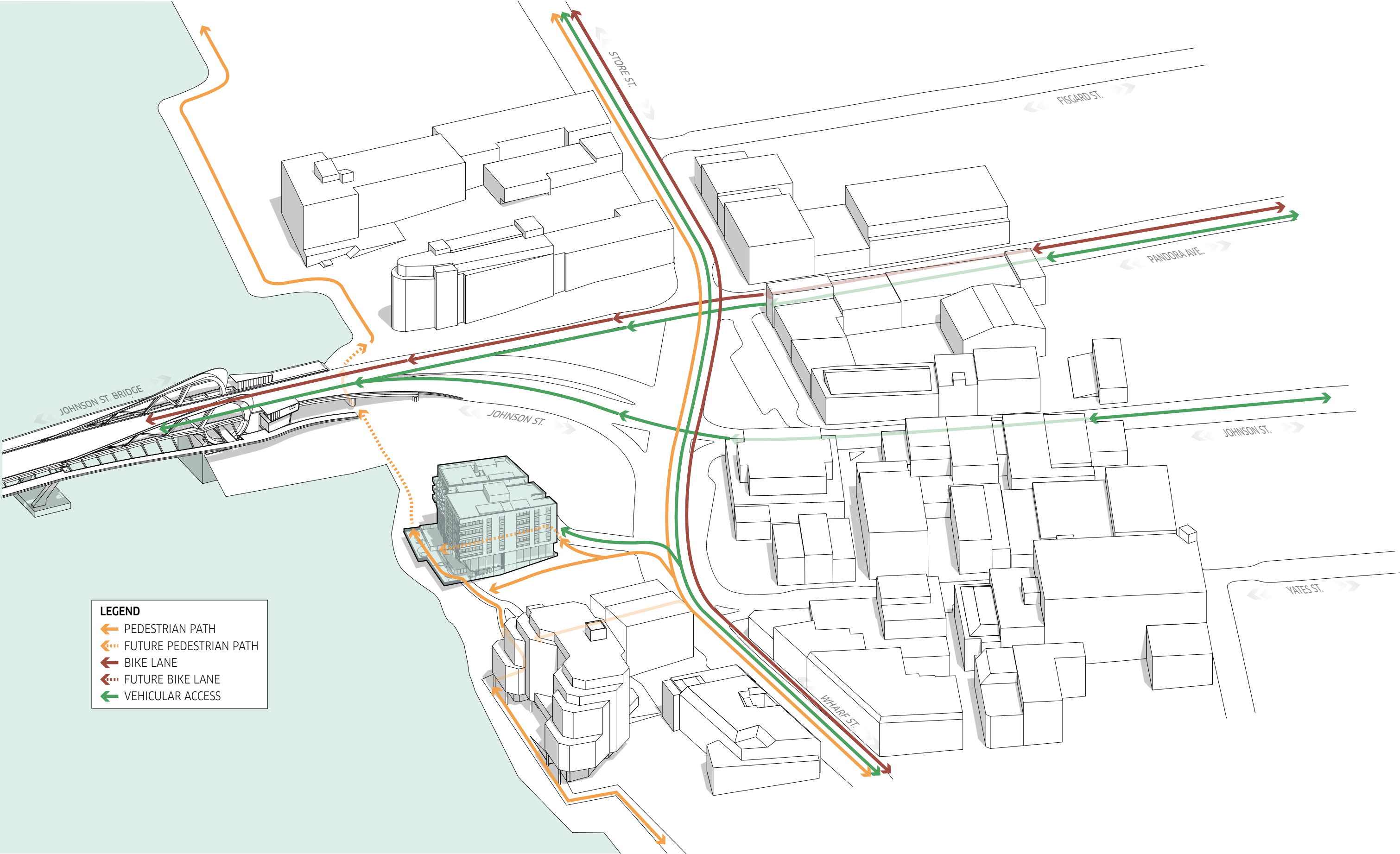
PUBLIC REALM:

With limited new site area in which to add new structure, the footprint of any of the new elements are kept to a minimum, allowing the heritage facades to dominate the pedestrian experience of the project. Where a narrow sloping lane currently separates the two existing warehouse buildings, a new semi-public alleyway is proposed to allow pedestrian connection from Wharf Street to the new extended public waterfront walkway system to the north. This alleyway is in keeping with the old town character of intimate alleys and courtyards. It allows two sides of the existing structures to become exposed over two levels and is activated by new openings to the commercial spaces and an elevator can be accessed by the public to further improve access to the waterfront walk directly through this site. The space will be gated after hours for security. A feature soffit material is proposed to tie together the alley and glazed atrium ceilings using a stamped tin-like panel system rendered in a reflective material to add interest and light to the views upwards below the new structures and draw pedestrians into the new alley space.

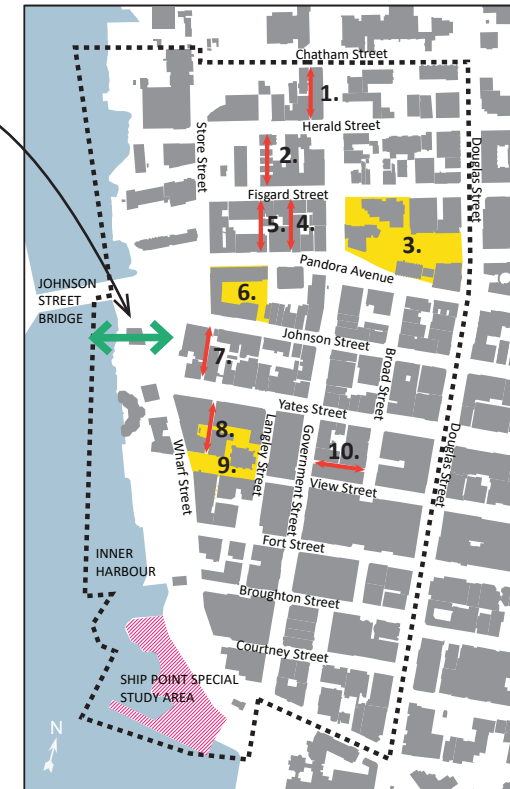
At the south side of the project bordering Reeson Park -where the new structure comes down to meet ground level- two highly-glazed active uses (a restaurant and residential lobby) are proposed to directly interface with the park, providing passive surveillance, illumination and an open relationship between new building and park. This highly glazed ground level of the project also allows for views of the south heritage facade through the building. A dramatic glazed atrium wraps the southwest corner of the Caire & Grancini building creating a unique experience of the heritage buildings and protects the heritage facade from the elements and vandalism.

Extension of the David Foster Way waterfront walk along the west edge of the project will continue this public realm asset one step closer to the bridge and future connection to Rock Bay. Providing a public connection in the middle of the project also allows a way to continue the public connection in the interim.



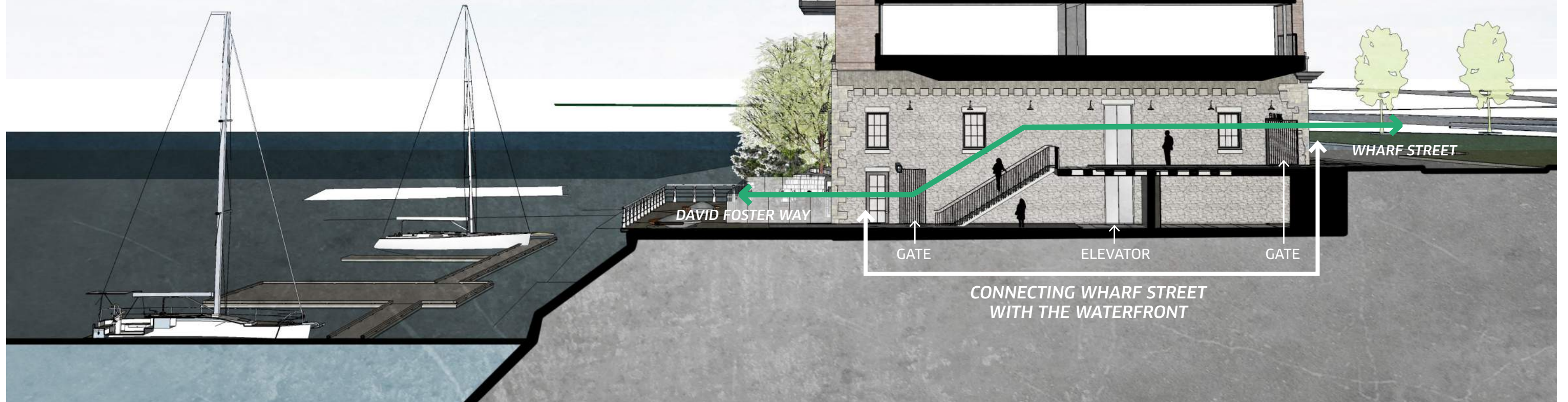


Courtyards, Squares and Alleys



- Legend**
- Square
 - Alley
- | | |
|----------------------|---------------------|
| 1. Lim Dat Alley | 6. Market Square |
| 2. Dragon Alley | 7. Waddington Alley |
| 3. Centennial Square | 8. Commercial Alley |
| 4. Fan Tan Alley | 9. Bastion Square |
| 5. Theatre Alley | 10. Trounce Alley |

**NEW PUBLIC
ALLEY**



ARCHITECTURE, MASSING & MATERIALS:

• EAST FACADE

This facade is broken down vertically into three primary elements to correspond to the two existing structures, and a new narrow addition to the south. Each is expressed slightly differently to respond in pattern and proportion to the masonry and openings of the existing buildings, and the south portion is expressed in a contemporary interpretation of the old town composition. Inboard balconies in keeping with Old-town precedents allow the masonry facades to reflect a historic streetscape pattern.

• NORTH FACADE

At the direction of the Planning Department, new openings have been added to Fraser Warehouse facade to better engage with the site to the north. The upper storeys also have openings and a provision for a public art element to address the approach from the bridge while anticipating a potential future development to the North.

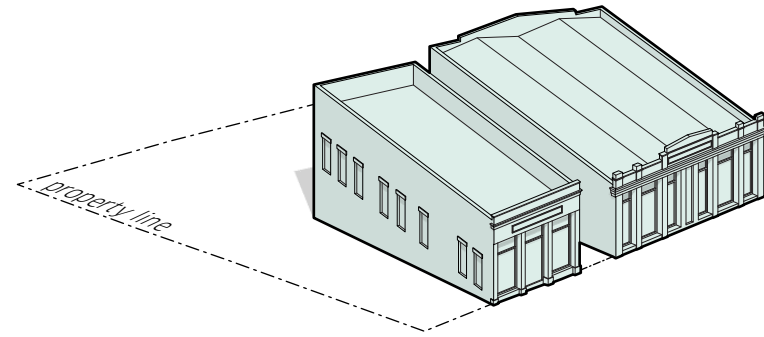
• WEST FACADE

The portion above the Fraser Warehouse expresses a more industrial feel for the balconies while featuring the historic 2-storey facade as a base. The public alleyway breaks this elevation into two distinct vertical masses with the south massing incorporating a transparent glazed portion at the base and a contemporary facade of terra-cotta cladding and punched openings above. The roof line of this southern portion is gently sloping to echo the angled parapet lines of the existing buildings and create a more varied profile to the development. Operable glazing and patio doors along the ground level of the west elevations allow an indoor-outdoor relationship of patios and public walkway with the new commercial uses at this lowest level. The differentiation of expression from east to west reflects the historic pattern of dual-frontage buildings on the harbour -one side addressing the working harbour, the other addressing Old-town.

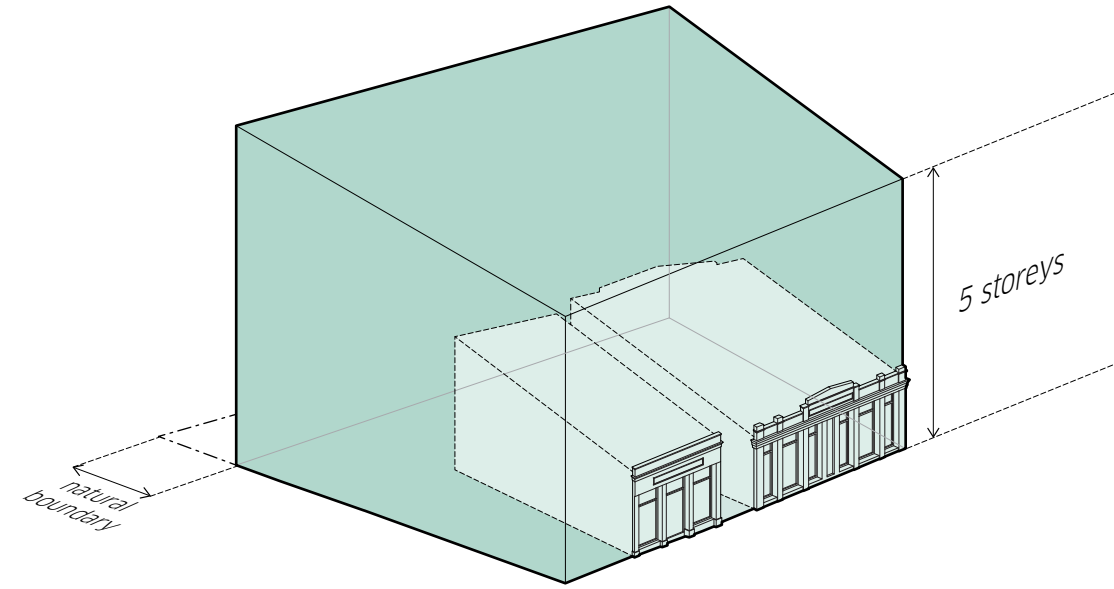
• SOUTH FACADE

Facing the park, this elevation orients balconies and glazing at the upper floors to engage the views of the park and harbour. The lower floors maximize glazing to provide overview of the park and visibility into the lower floors and reveals the historic facade contained within.

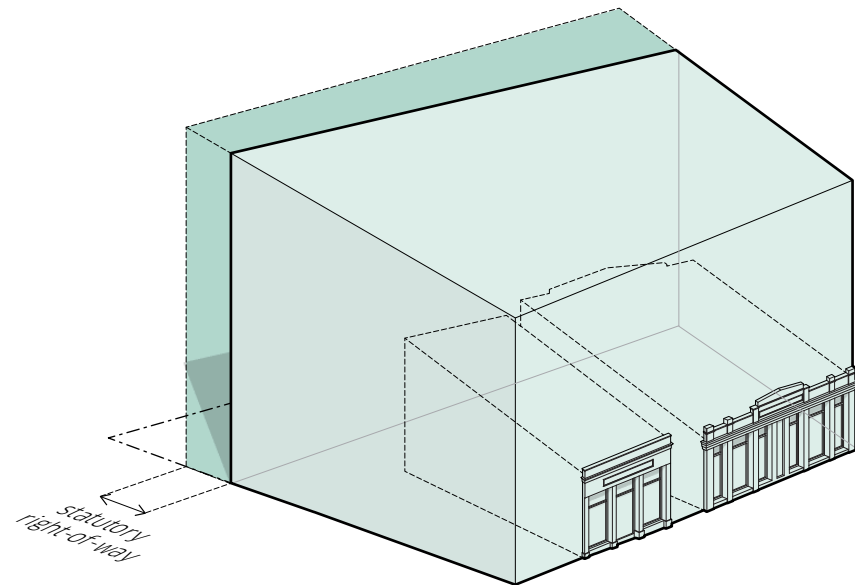




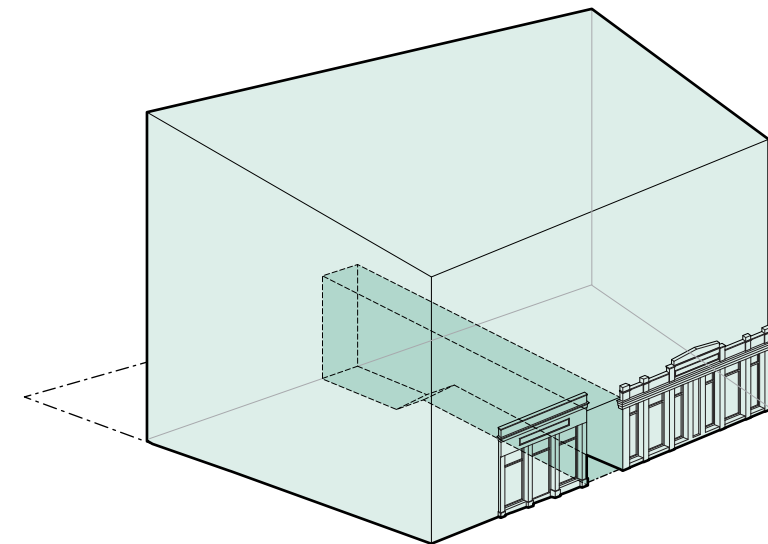
NORTHERN JUNK
EXISTING HERITAGE BUILDINGS



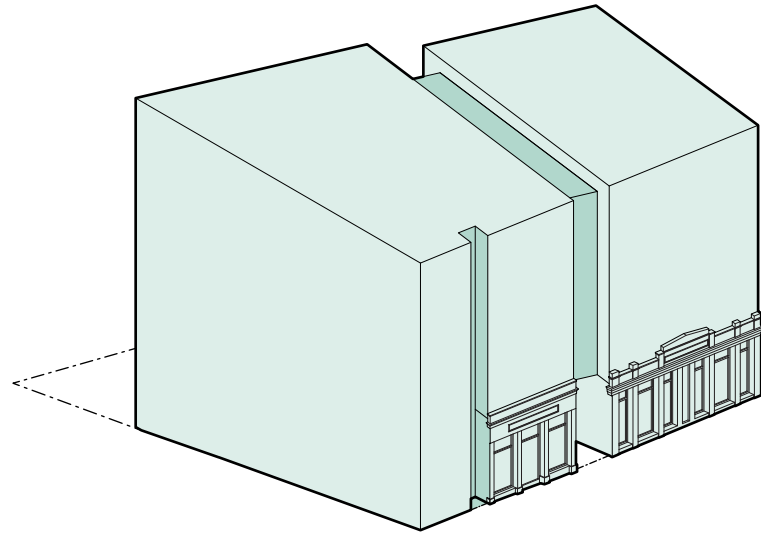
APPLY MAXIMUM FSR
TO NATURAL BOUNDARY



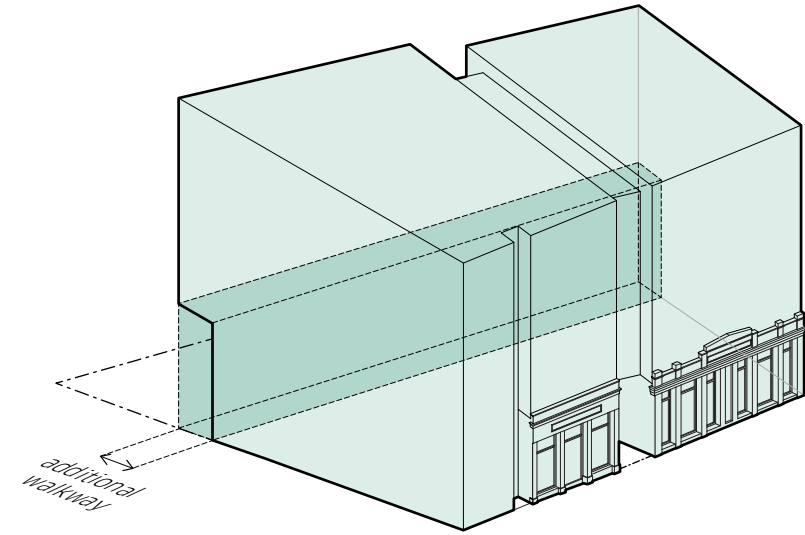
ALLOW FOR WATERFRONT ACCESS
STATUTORY RIGHT-OF-WAY



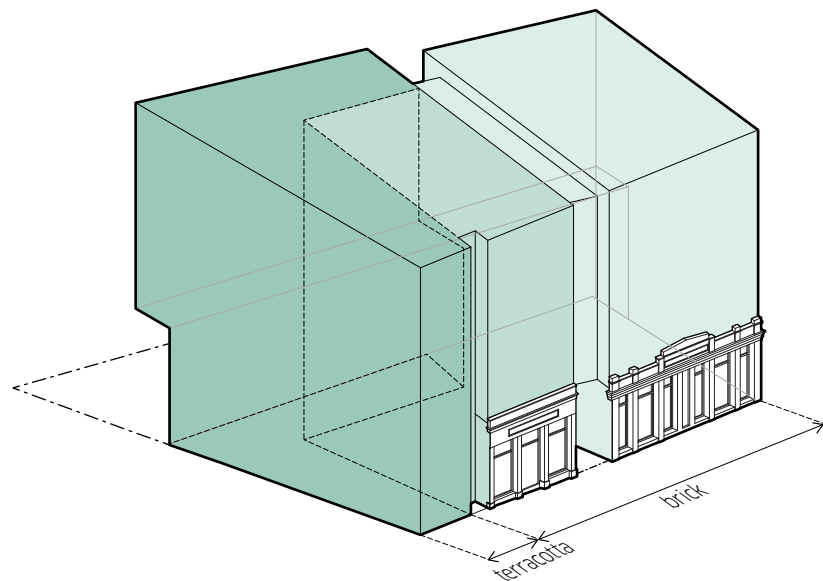
ADD PEDESTRIAN FRIENDLY ALLEY



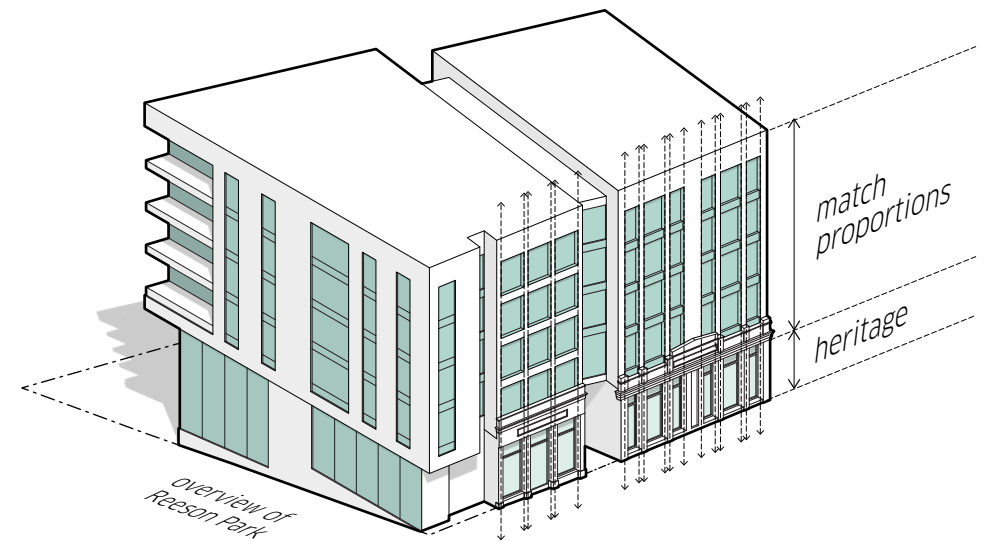
CARRY FORM FROM EXISTING HERITAGE
UPWARD TO INFORM MASSING & MAINTAIN
TRADITIONAL HERITAGE STREETScape PATTERN



REMOVE MASSING TO ALLOW
LARGER WATERFRONT WALKWAY &
EXPOSE HERITAGE FACADES



MATERIALS HIGHLIGHT HERITAGE
ORIGINS & DIFFERENTIATE MASSING



ARTICULATION INFORMED BY
HERITAGE PROPORTIONS



CONTEMPORARY ADDITION



HERITAGE WITHIN GLAZING



BRICK RELIEF



CONTEMPORARY CORNICE



HERITAGE ADAPTATION



HERITAGE ADAPTATION



PUBLIC ART



CONTEMPORARY CORNICE





CONTEXT **SW**



CONTEXT **SE**



CONTEXT **NW**



CONTEXT **NE**



STUDY **BLANK**



STUDY **1**



STUDY **2**



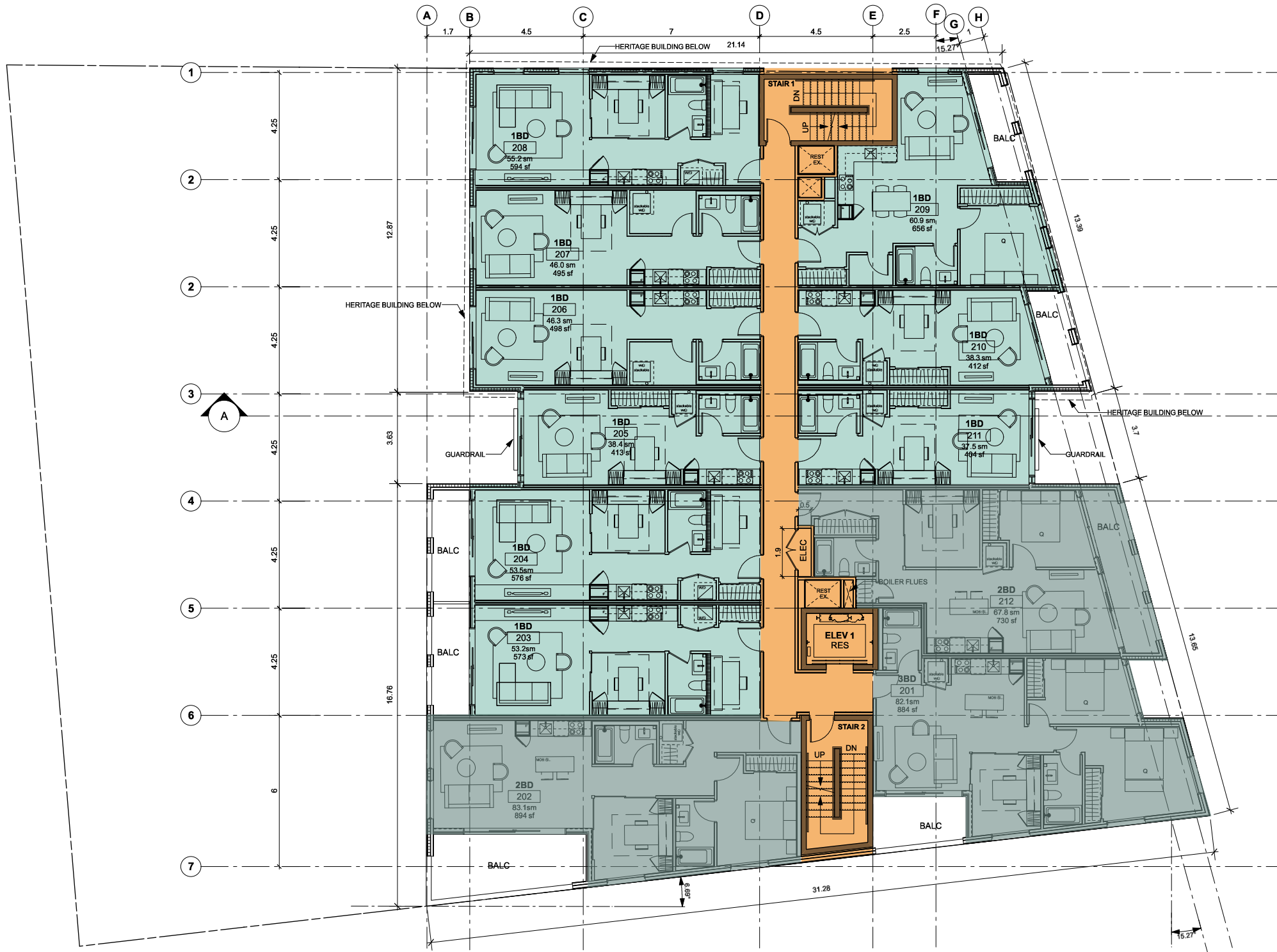
STUDY **3**

03 ARCHITECTURAL PACKAGE



LEGEND

- RESIDENTIAL SUITE
- FAMILY SUITE
- COMMERCIAL AREA
- CIRCULATION/SERVICE SPACE



LEVEL 02 AREAS		
SUITE NUMBER	SUITE TYPE	SUITE AREA
201	3BD	82 sm
202	2BD	83 sm
203	1BD	53 sm
204	1BD	54 sm
205	1BD	38 sm
206	1BD	46 sm
207	1BD	46 sm
208	1BD	55 sm
209	1BD	61 sm
210	1BD	38 sm
211	ST	38 sm
212	2BD	68 sm
Total Suite Area		662 sm
		GROSS
Total Gross Area		738 sm
EFFIC		90%

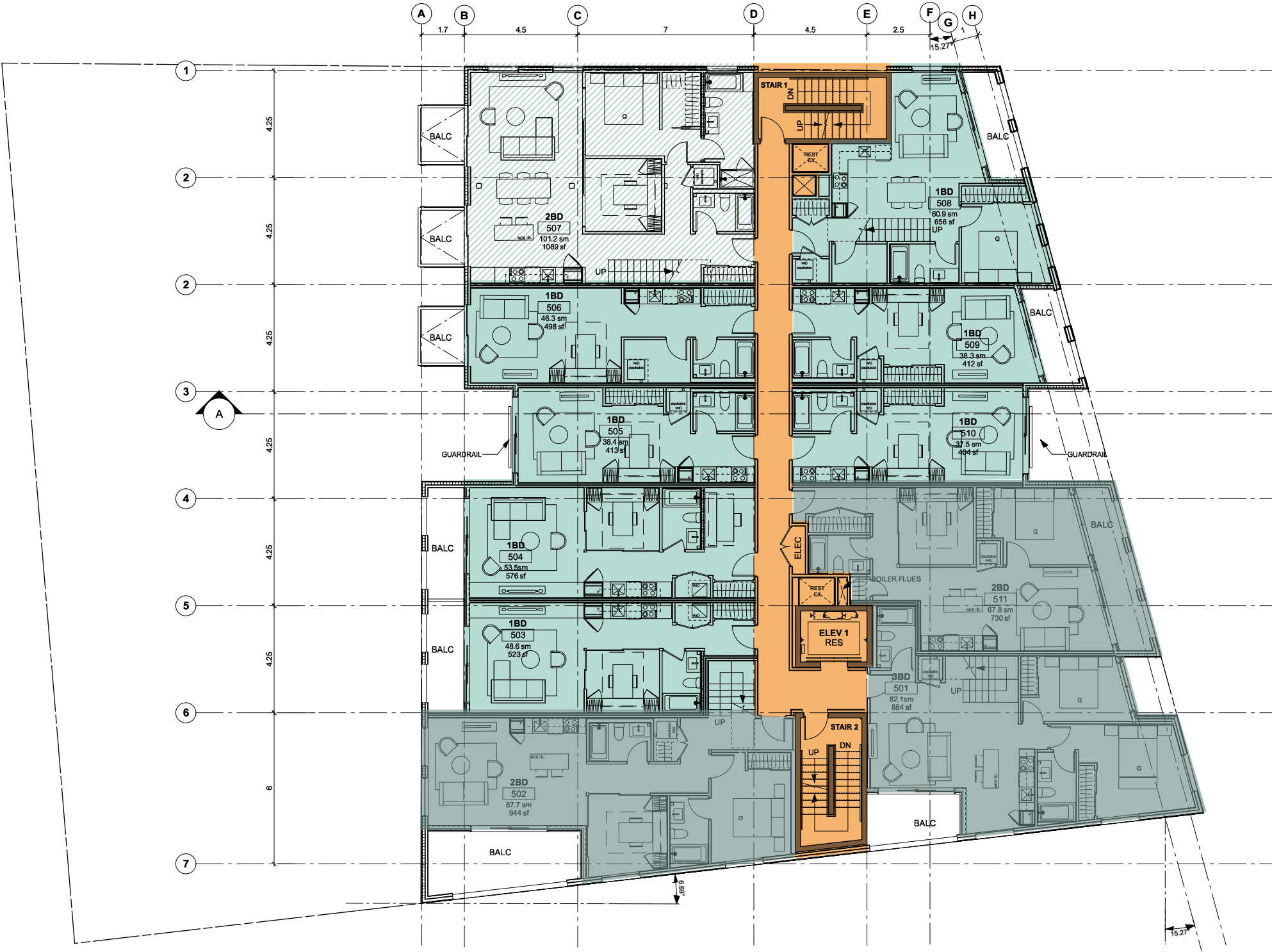
LEGEND

RESIDENTIAL SUITE

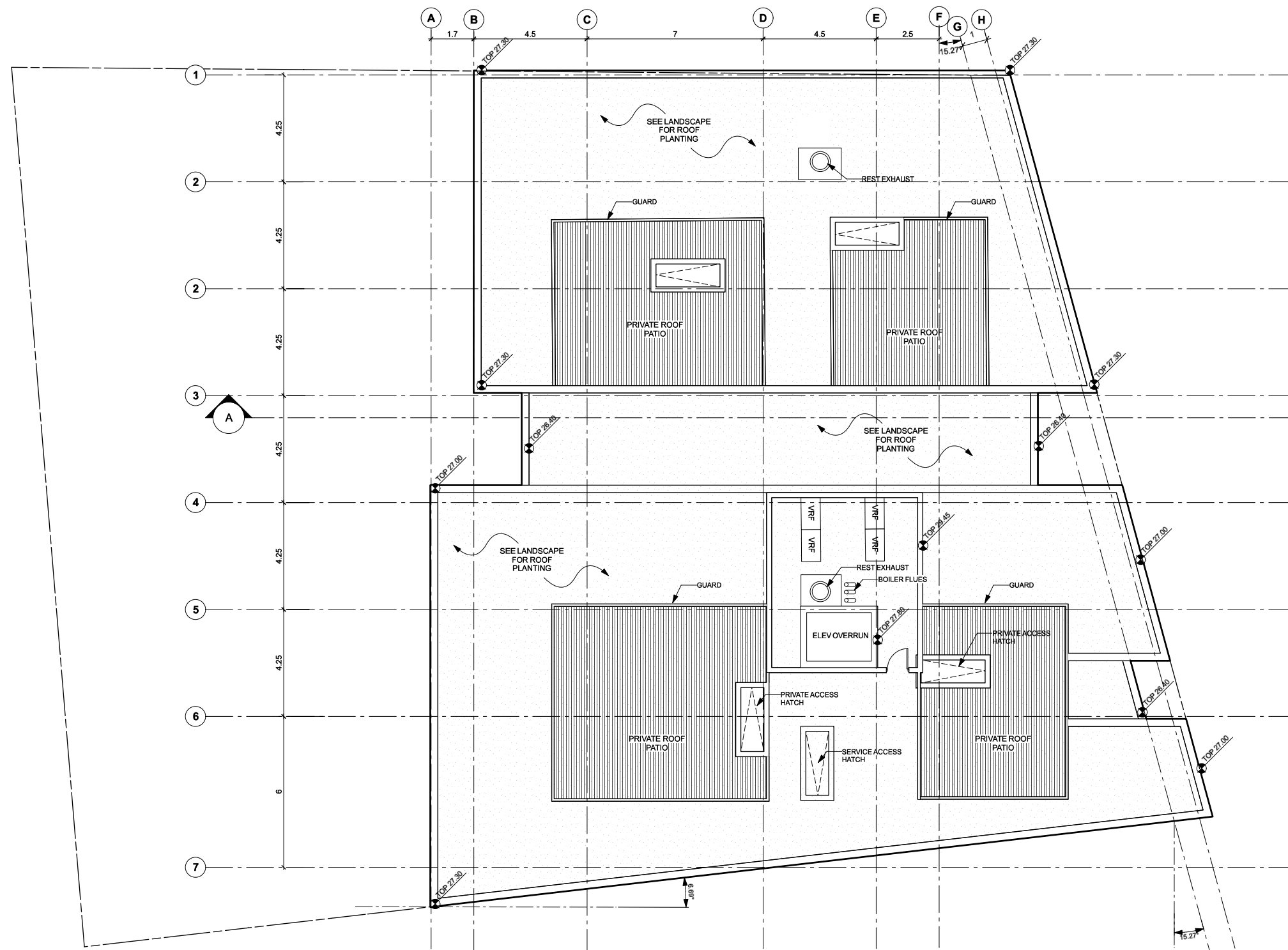
FAMILY SUITE

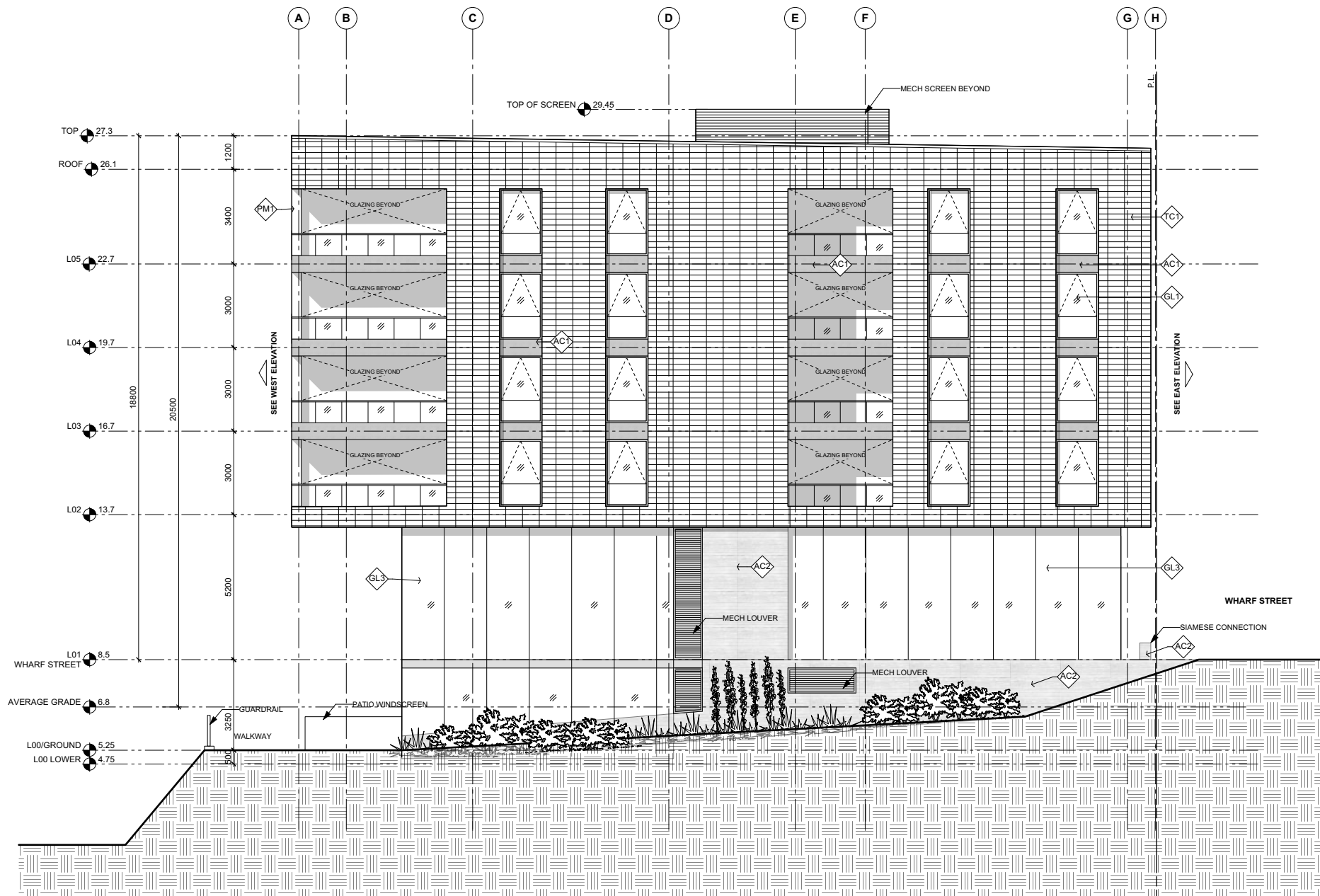
COMMERCIAL AREA

CIRCULATION/SERVICE SPACE

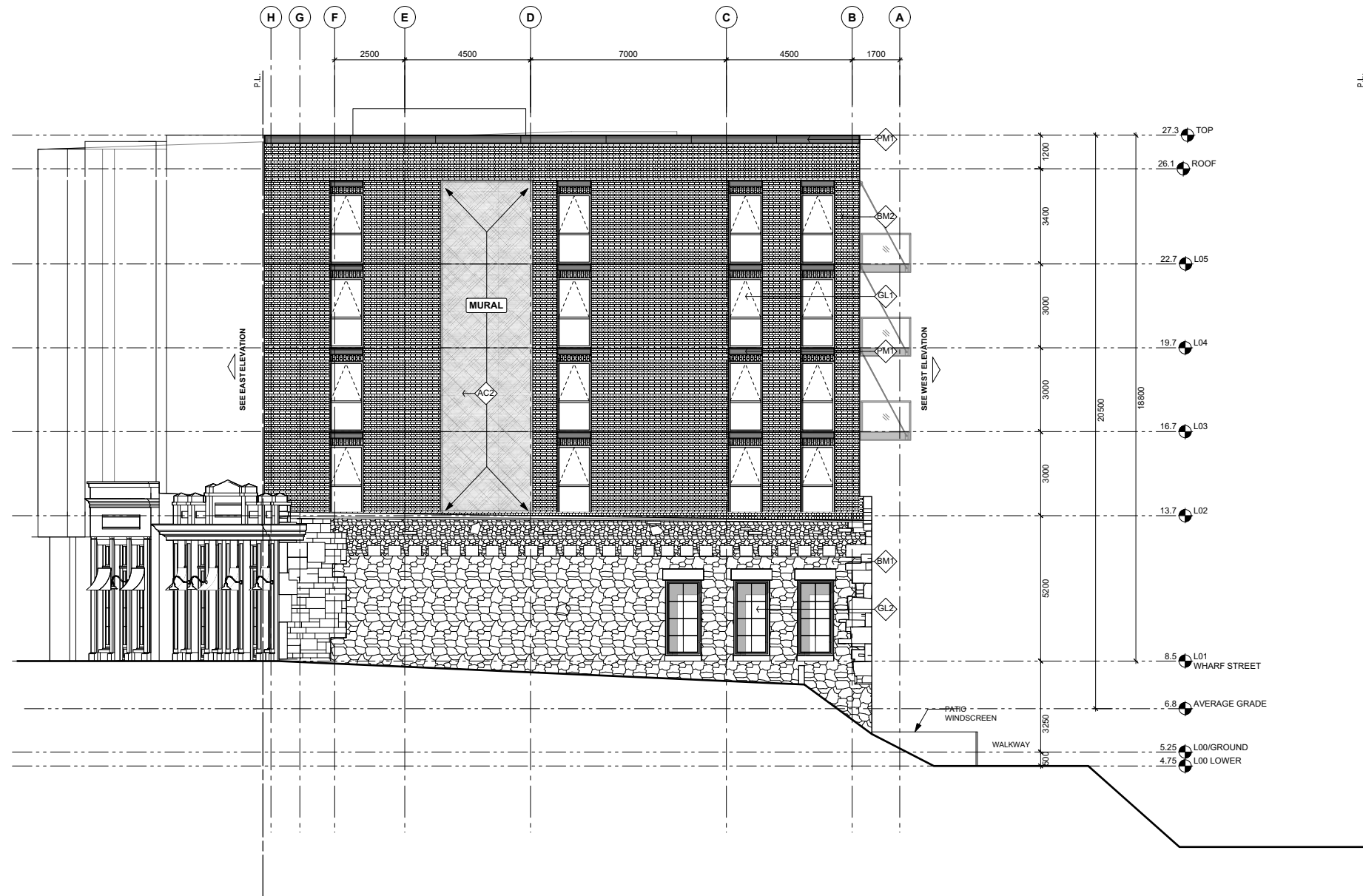


LEVEL 05 AREAS		
SUITE NUMBER	SUITE TYPE	SUITE AREA
501	3BD	82 sm
502	2BD	88 sm
503	1BD	49 sm
504	1BD	54 sm
505	1BD	38 sm
506	1BD	46 sm
507	2BD	101 sm
508	1BD	61 sm
509	1BD	38 sm
510	ST	38 sm
511	2BD	68 sm
Total Suite Area		662 sm
		GROSS
Total Gross Area		738 sm
EFFIC		90%

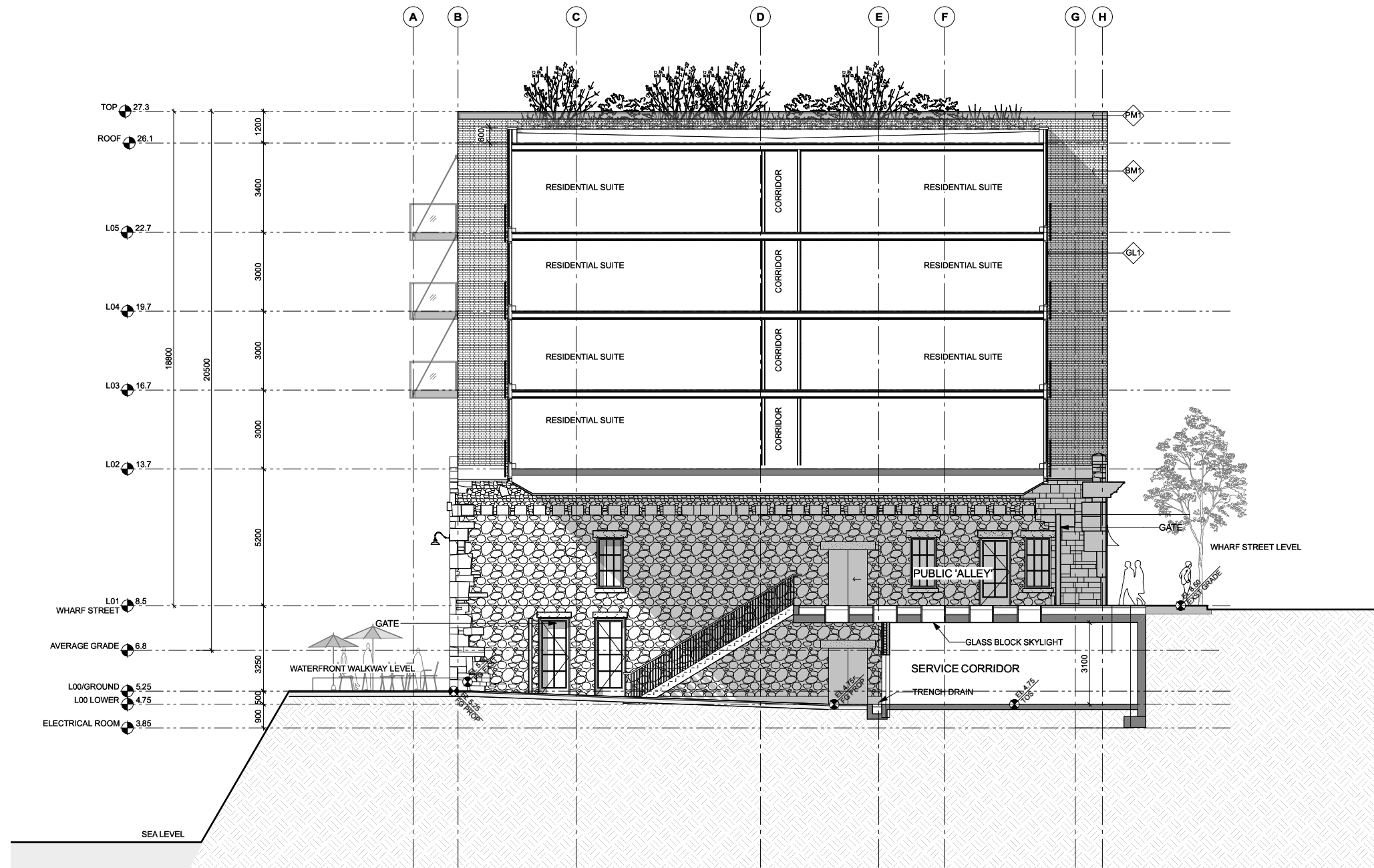












04 DIGITAL RENDERINGS









4.1 | RENDERING 3







05 LANDSCAPE PACKAGE

The landscape design associated with the Northern Junk building creates a functional and vibrant urban waterfront space. The public will benefit from the extended connection of the David Foster Way and proximities to Reeson Park and Johnson Street Bridge Public Realm.

Wharf Street Connection

The cast in place concrete sidewalk follows the requirements outlined in the Downtown Public Realm Plan and Streetscape Standards for the district of Inner Harbour. There isn’t room between the existing buildings and curb line to include street trees. By not shifting the curb to achieve the necessary space we are able to protect the mature trees opposite the building.

The hardscape paving in the passage between the buildings uses the same rhythm as the jointing in the sidewalk to create unity of the two spaces. The passageway also benefits from the glass tiles that create a light well into the spaces below. In-ground lighting along the edges provides safe lighting levels to prevent CTPED related issues. A gate, located in line with the building faces and designed with historical reference, can be used to secure the passageway after business hours.

Reeson Park Interface

The building design promotes an eyes-on-the park relationship of the CRU, the lobby and the residential units above with Reeson Park. This is not only a benefit to the residents but provides a significant CPTED improvement as the sunken park is not in view from the street which currently promotes undesirable behavior. This design proposes a low park planter along the building with low plant material and uniformly spaced columnar trees to preserve the views while at the same time softening the edge and building face. This will need to be reviewed in the context of the ongoing Reeson Park design process. We also propose shifting the bleacher seating slightly towards the water so that the Wharf Street sidewalk can extend directly across the park.

David Foster Way

The public extension of David Foster Way provides the required 5-meter width. The walkway is constructed from heavy timber members that evoke the industrial historical significance of the site. The pattern expressed on the timber decking relates to the shoreline below and makes pedestrians aware of the connection between the ocean and the industrial history. Emphasizing this relationship between the built and natural environment are two large precast “stone” seating features. They also provide a place to rest, wait for a table at the restaurant or simply enjoy the view.

The two outdoor patios are surfaced with stone pavers that create significant visual distinction between private and public spaces and also carry the historical connotation. These patios will provide animation along the David Foster Way and highly sought after because. The comfortable atmosphere of the outdoor dining patios is created by discrete glass enclosures defining the spaces and protecting from the ocean winds. It is further emphasized by catenary lighting that defines the space with open canopy and provides soft ambient light.

The lower level of the passageway between the historical buildings uses heavy timber paving to create visual unity between the passageway and David Foster Way. In-ground lighting along the edges provides safe lighting levels to prevent CTPED related issues. As at the top on Wharf Street, a gate In line with the buildings provides after business hour security.

Rooftop

The rooftop includes an extensive sedum, grass and perennial green roof and small private patios. The patios are paved with hydropressed pavers to allow for ease of removal.

For replacement and access to the slab for maintenance. A large cast in place planter is located in the middle of the roof. The plant palette includes native trees and native adapted plants with a variety of bloom periods and textures to reduce the need for watering while delivering all season interest.

Sustainability

Locally sourced stone pavers, manufactured timber and paving slabs have been selected as the paving material for their durability. A high efficiency, fully automated drip irrigation system with rain sensor will ensure healthy plant growth while keeping water use to a minimum.

The green roof improves air quality, provides significant areas of planted space which will contribute to the reduction of heat island effect, reduce the urban storm water runoff and Increases the habitat area along the shoreline.

TREE PROTECTION GENERAL NOTES

A. EXCAVATION AROUND TREES

- EXCAVATION WITHIN DRIP LINE OF TREES ONLY WHERE INDICATED ON PLANS AND AS DIRECTED BY THE CONSULTANT.
- DURING ANY EXCAVATION WITHIN THE DRIP LINE OF A TREE THE CONTRACTOR SHALL EXCAVATE AROUND TREE ROOTS AS DIRECTED BY THE CONSULTANT. DO NOT CUT TREE ROOTS UNLESS DIRECTED BY THE CONSULTANT.
- TREES AND OTHER DESIRABLE VEGETATION TO BE TOTALLY FENCED BY 1.8M (6'-0") HIGH SEMI-PERMANENT CHAIN-LINK FENCING. FENCING TO BE MAINTAINED FOR THE DURATION OF THE PROJECT.

B. EXCAVATION FOR NEW CONSTRUCTION WITHIN THE DRIP LINES OF TREES

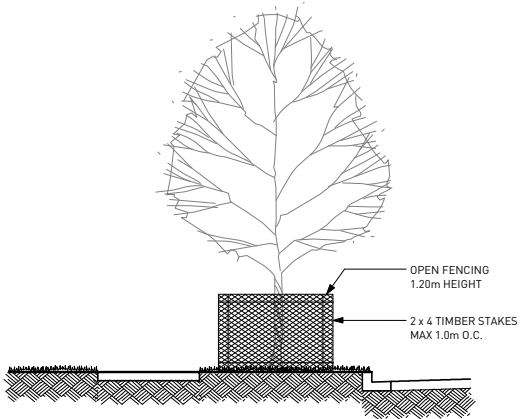
- HAND EXCAVATE TO MINIMIZE DAMAGE TO ROOT SYSTEMS.
- USE NARROW TINE SPADING FORKS TO PROBE AND COMB SOIL TO EXPOSE ROOTS.
- RELOCATE ROOTS INTO BACKFILL AREAS WHENEVER POSSIBLE. IF LARGE MAIN LATERAL ROOTS ARE ENCOUNTERED, EXPOSE BEYOND EXCAVATION LIMITS AS REQUIRED TO BEND AND RELOCATE WITHOUT BREAKING.

C. UTILITY TRENCHING WITHIN THE DRIP LINES OF TREES

- TUNNEL UNDER AND AROUND ROOTS BY HAND DIGGING.
- DO NOT CUT MAIN LATERAL ROOTS.
- CUTTING OF SMALLER ROOTS THAT INTERFERE WITH INSTALLATION OF NEW WORK SHALL BE DONE WITH CLEAN SHARP TREE PRUNING TOOLS.
- ROOTS THAT ARE ENCOUNTERED IMMEDIATELY ADJACENT TO THE LOCATION OF NEW CONSTRUCTION AND ARE TOO DIFFICULT TO RELOCATE SHALL BE CUT 15cm (6") BACK FROM NEW CONSTRUCTION. USE CLEAN SHARP TREE PRUNING TOOLS.

D. PROTECTION OF EXPOSED ROOTS

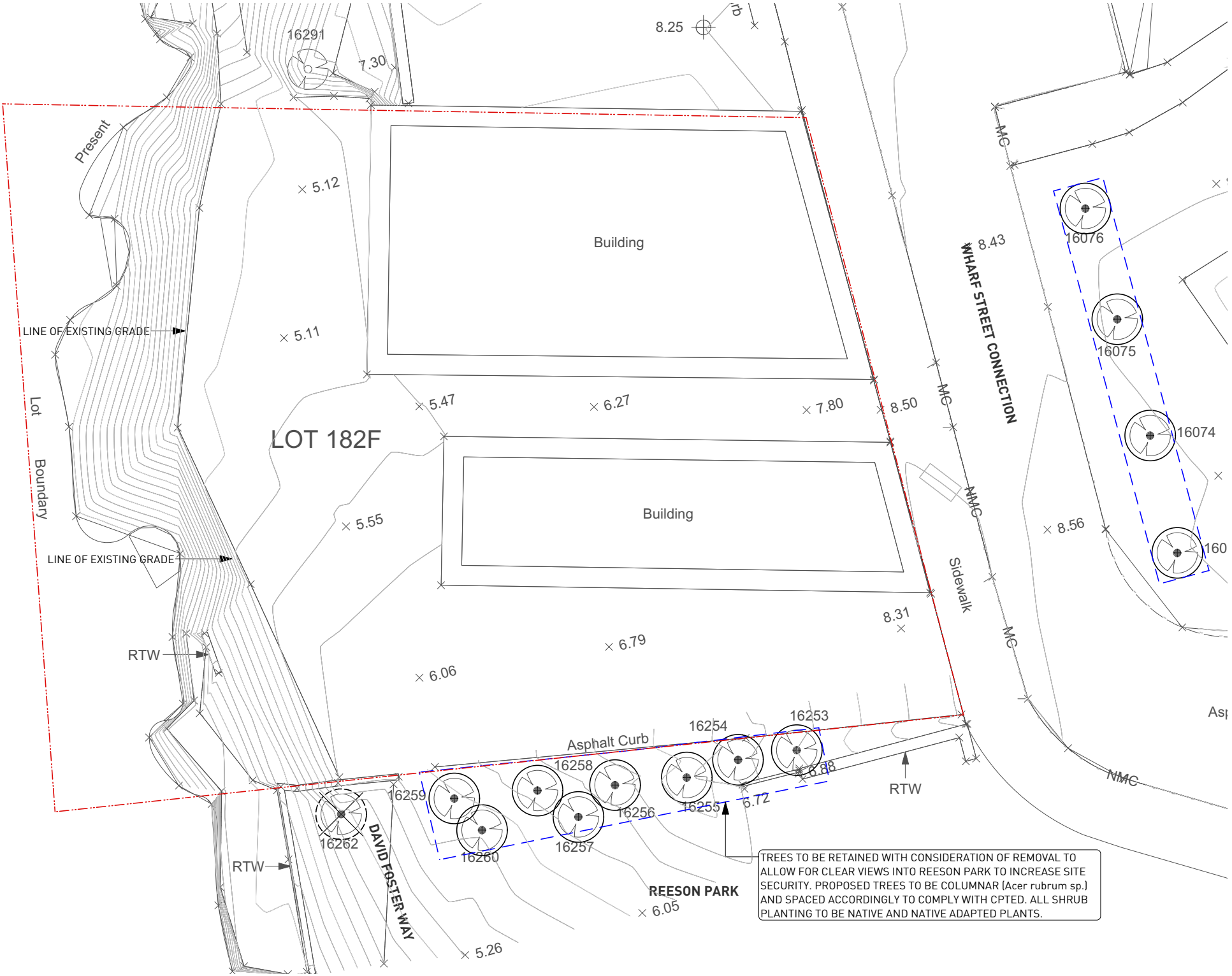
- DO NOT ALLOW EXPOSED ROOTS TO DRY OUT PRIOR TO PLACEMENT OF PERMANENT COVER. PROVIDE ONE OF THE FOLLOWING TEMPORARY REMEDIAL MEASURES:
 - PROVIDE TEMPORARY EARTH COVER. MAINTAIN MOISTURE.
 - PACK WITH WET PEAT MOSS. MAINTAIN MOISTURE.
 - PACK WITH FOUR LAYERS OF WET UNTREATED BURLAP. MAINTAIN MOISTURE.
- TEMPORARILY SUPPORT AND PROTECT EXPOSED ROOTS FROM DAMAGE UNTIL PERMANENTLY RELOCATED AND COVERED WITH BACKFILL.
- WATER PUDDLE BACKFILL AROUND ROOTS TO ELIMINATE VOIDS AND AIR POCKETS.



1 TREE PROTECTION BARRIER FENCING DETAIL
 NTS

TREE MANAGEMENT LEGEND

	REMOVED DECIDUOUS TREE
	RETAINED DECIDUOUS TREE
	TREE PROTECTION FENCING



LAYOUT AND MATERIALS GENERAL NOTES

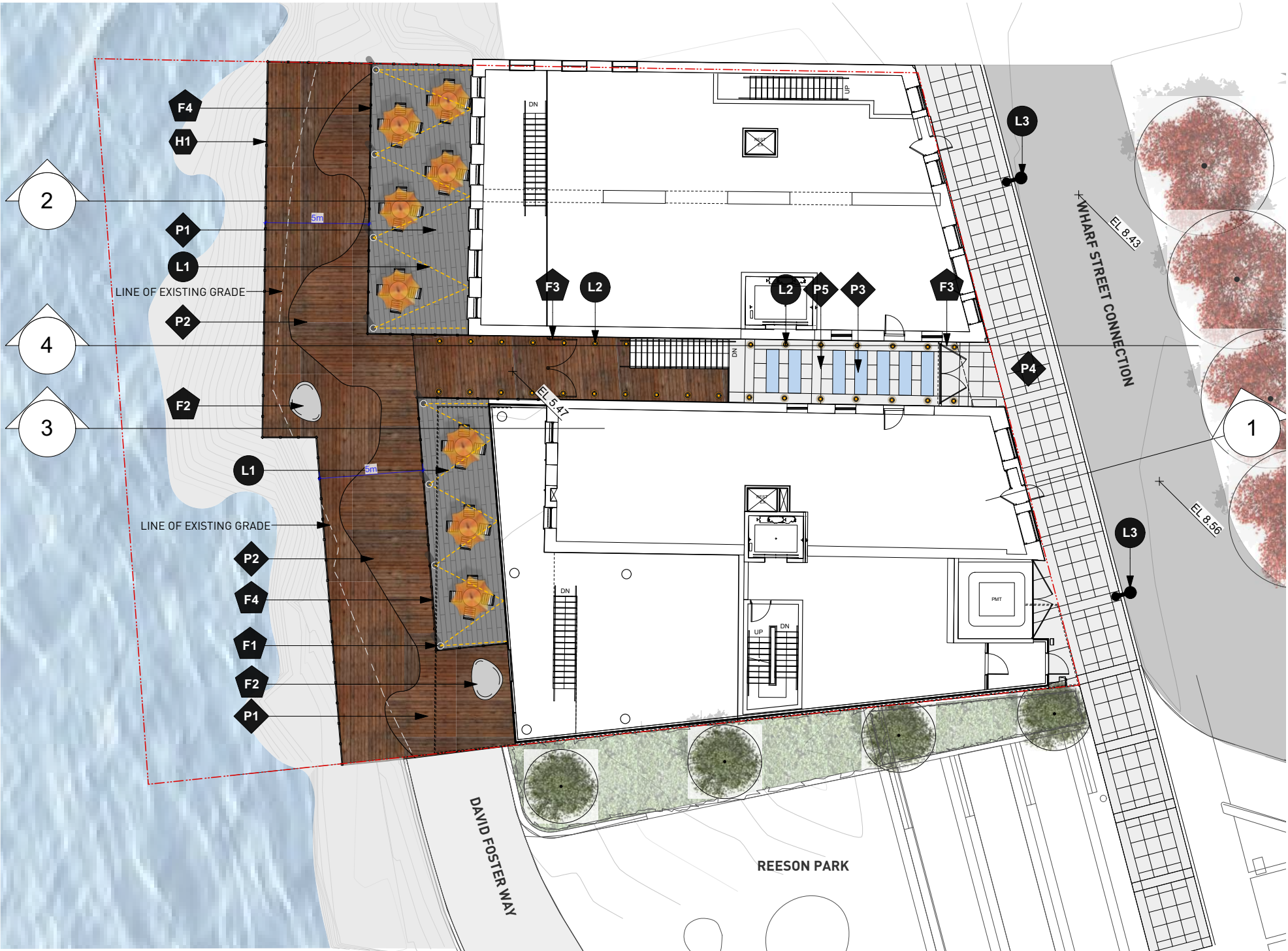
- 1. DO NOT SCALE DRAWING. LAYOUT AS PER DIMENSIONS NOTED ON LANDSCAPE PLANS. REPORT ANY DISCREPANCIES TO CONSULTANT FOR REVIEW AND RESPONSE.
- 2. LAYOUT AND MATERIALS DRAWINGS ARE TO BE READ IN CONJUNCTION WITH LANDSCAPE SPECIFICATIONS.
- 3. LANDSCAPE DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ARCHITECTURAL AND ENGINEERING DRAWINGS. REPORT ANY DISCREPANCIES TO CONSULTANT FOR REVIEW AND RESPONSE.
- 4. VERIFY ALL DIMENSIONS WITH FIELD CONDITIONS. REPORT ANY DISCREPANCIES TO CONSULTANT FOR REVIEW AND RESPONSE.
- 5. EXTERIOR LIGHTING SHOWN ON LANDSCAPE PLANS IS FOR REFERENCE ONLY. LIGHTING INFORMATION REFERENCED ON LANDSCAPE DRAWINGS IS TO BE READ IN CONJUNCTION WITH ELECTRICAL ENGINEER'S DRAWINGS.
- 6. REFERENCE CIVIL ENGINEER'S DRAWINGS FOR LAYOUT OF ROAD CURBS AND GUTTERS.

HARDSCAPE MATERIALS LEGEND	
KEY	DESCRIPTION
H1	Metal Guard Rail As per C.O.V Standards
H3	Maintenance Strip
H4	Parapet Wall per Arch Dwg.

LIGHTING LEGEND	
KEY	DESCRIPTION
L1	Catenary Lighting
L2	Inground Up Lights
L3	Proposed Street Lights

SITE FURNISHING LEGEND	
KEY	DESCRIPTION
F1	Catenary Light Post
F2	Metalco Stone Free Shape Seat
F3	Metal Gate
F4	Glass Guardrail Wind Protection Refer to Architectural

PAVING MATERIALS LEGEND	
KEY	DESCRIPTION
P1	Stone Paving Type 1
P2	Timber Decking
P3	Light Wells
P4	CIP Concrete Sidewalk As per C.O.V Street Standards
P5	CIP Concrete
P6	Hydrapressed Pavers



LAYOUT AND MATERIALS GENERAL NOTES

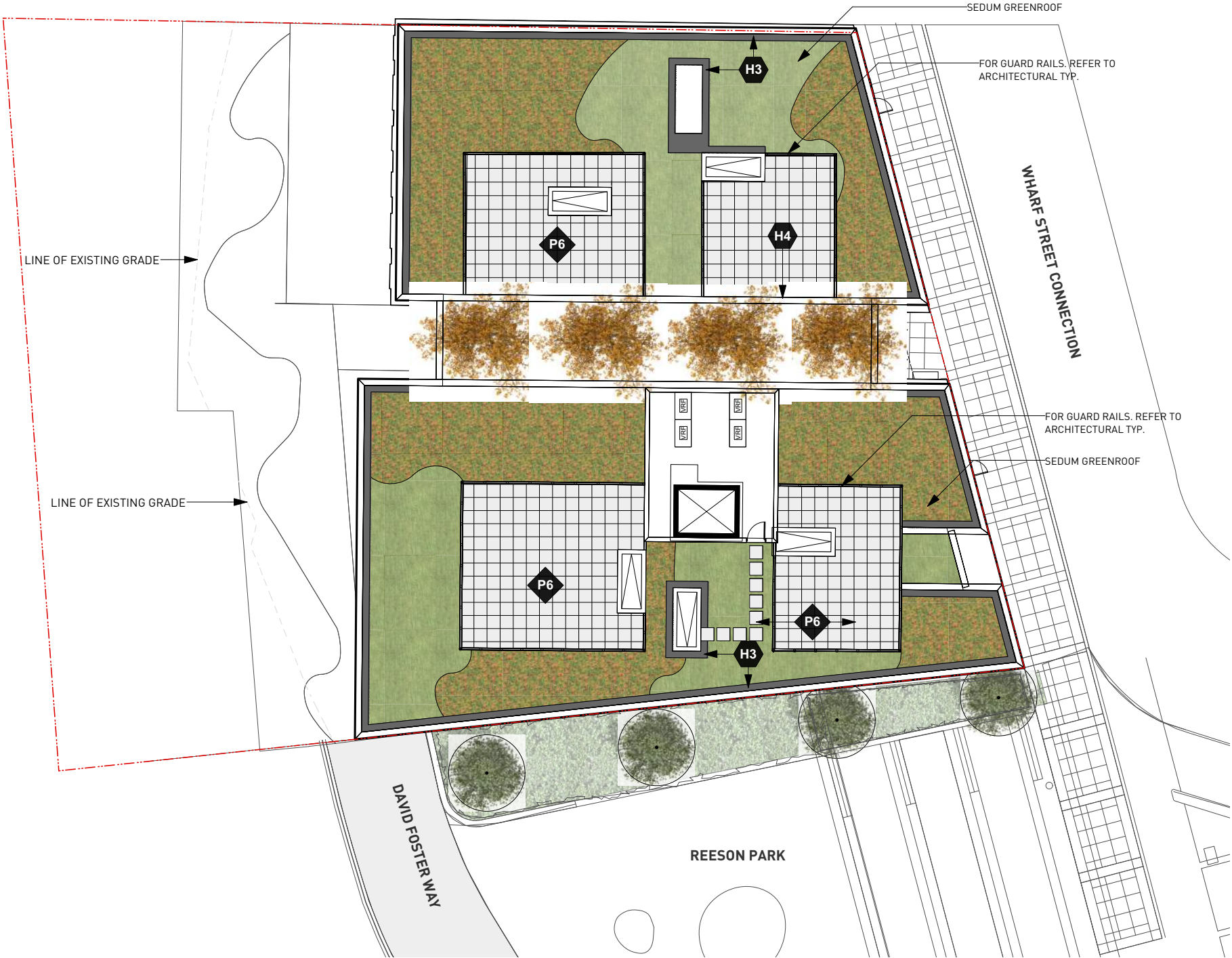
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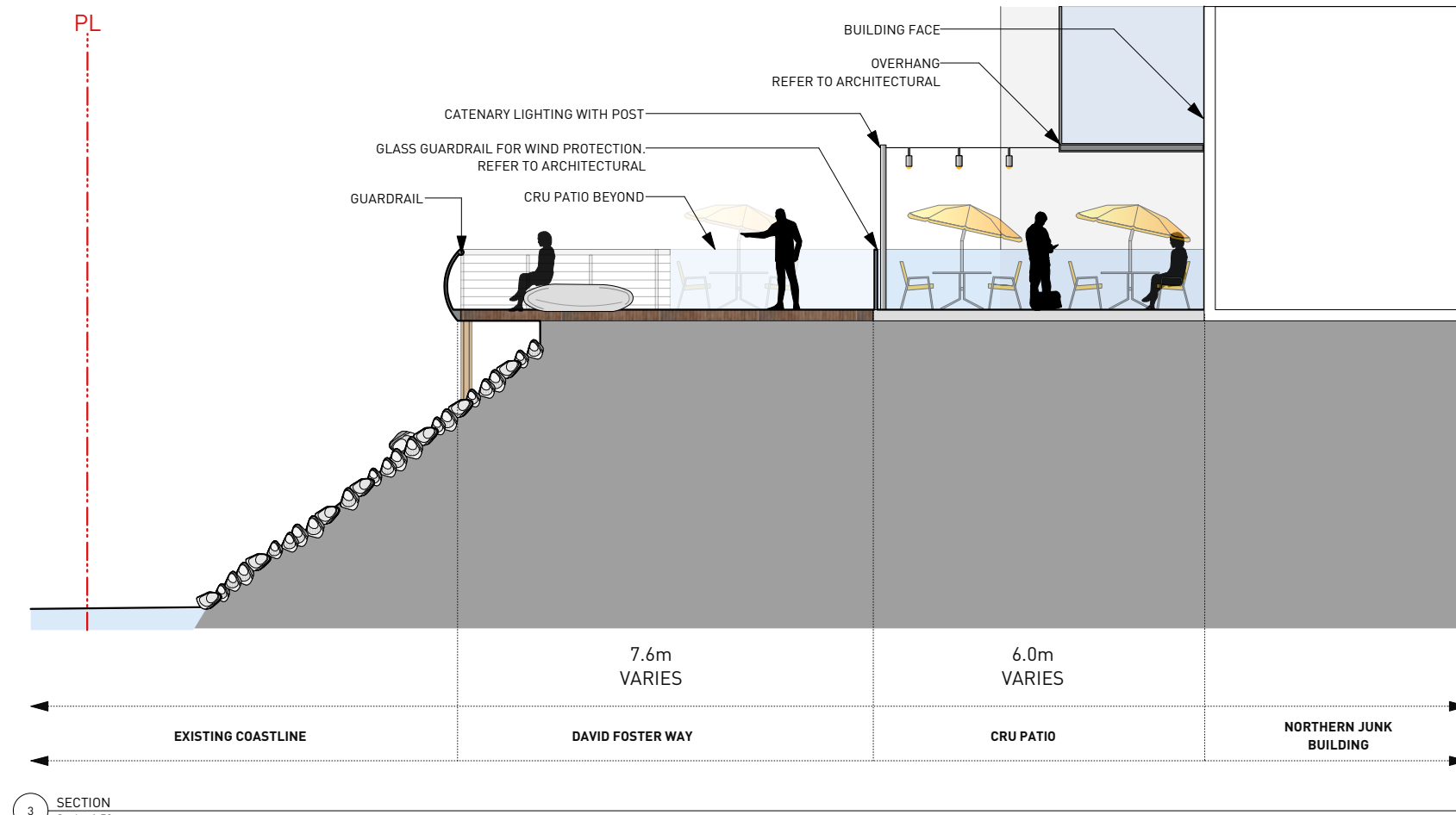
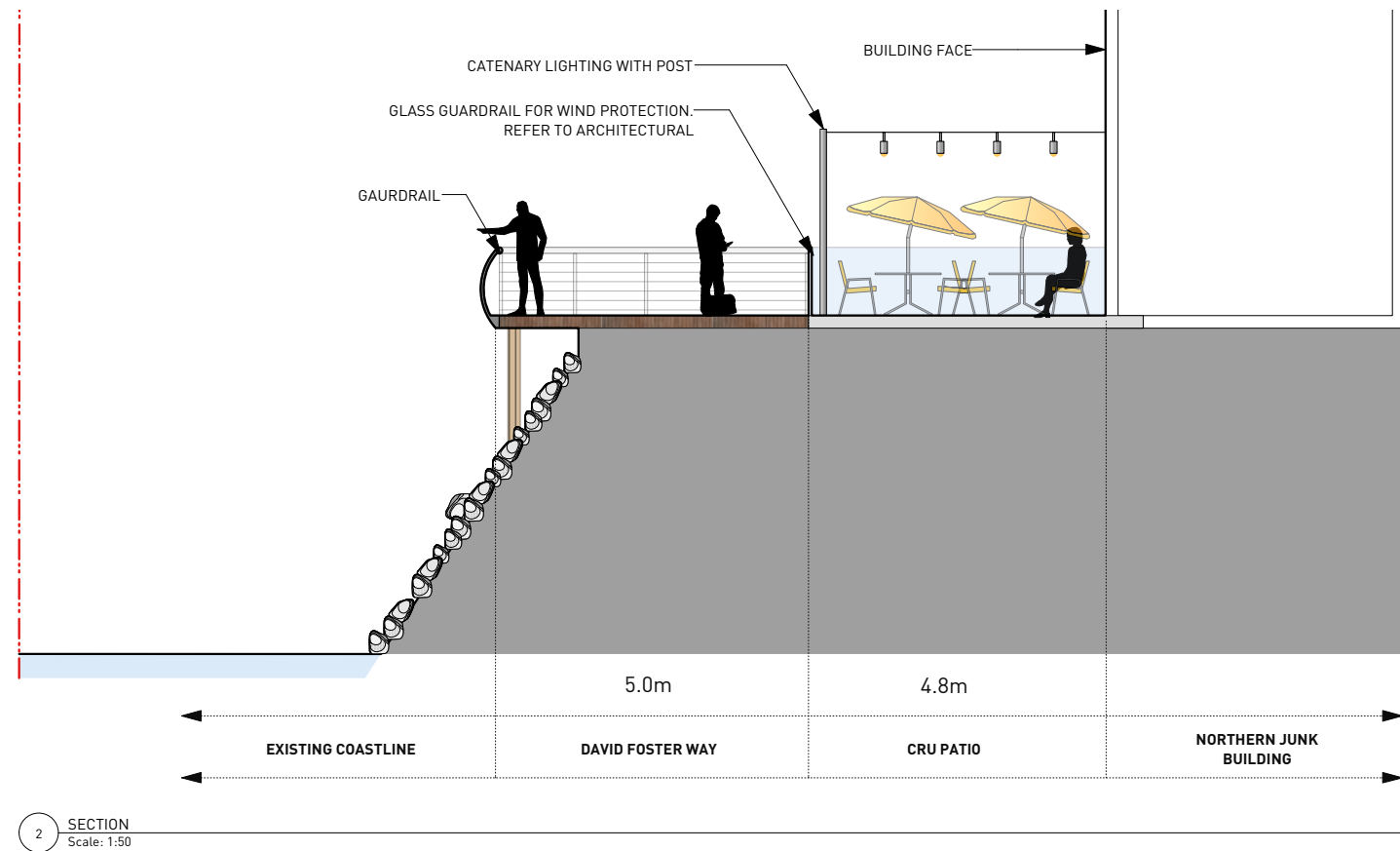
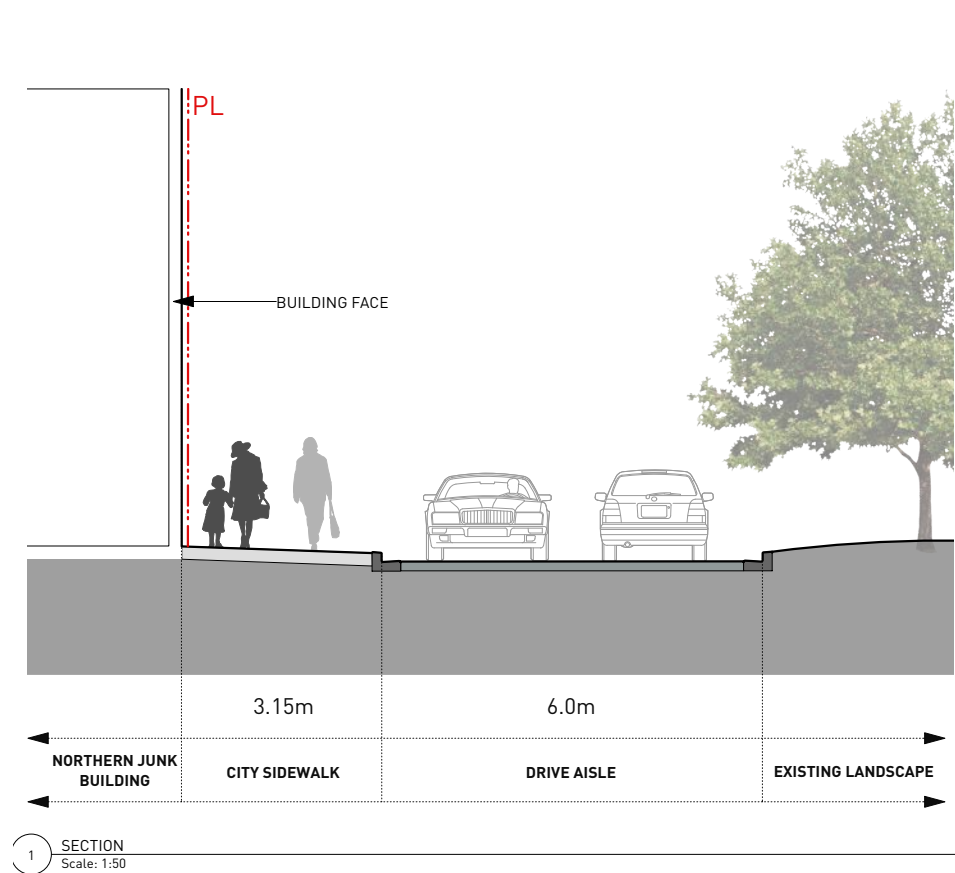
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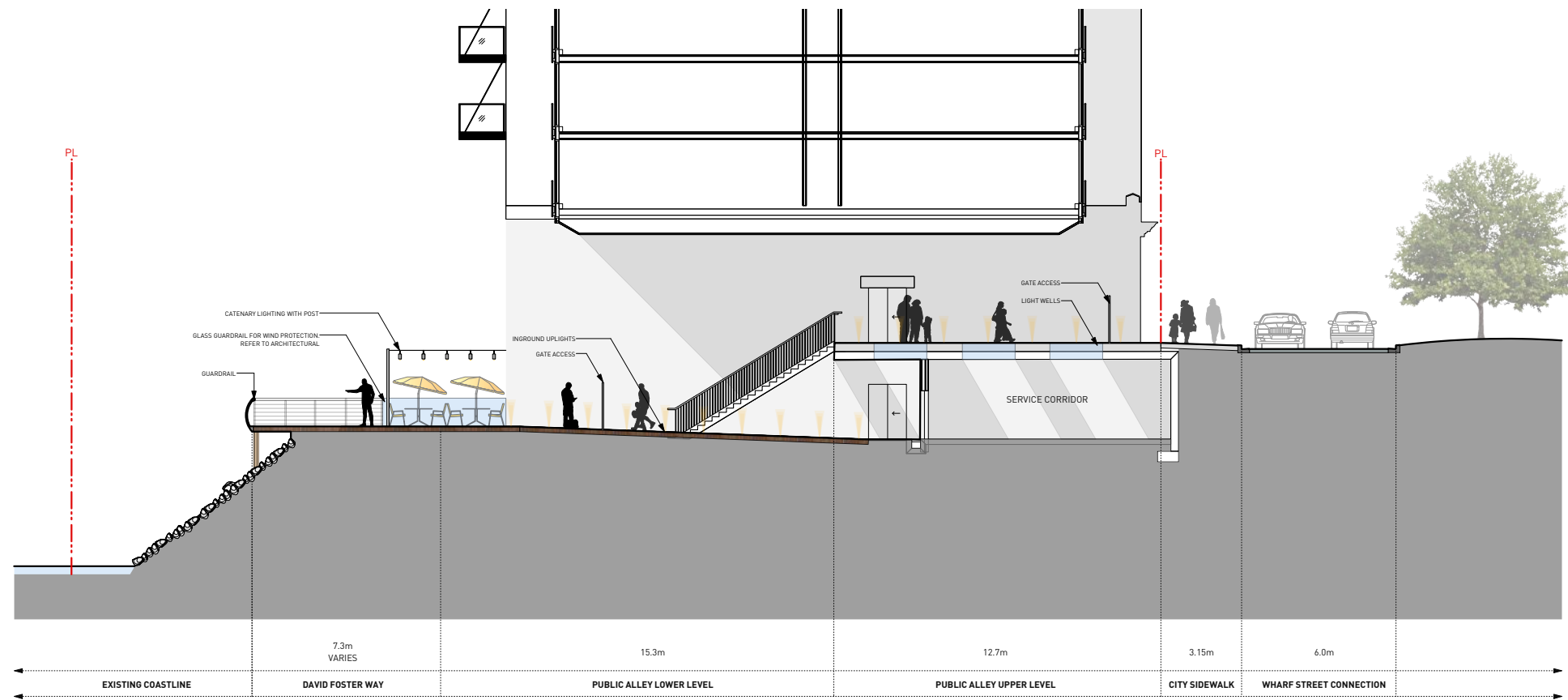
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P6	Hydrapressed Pavers







4 SECTION
Scale: 1:100