



A

Heritage Strategy

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Front cover photograph:

Roundhouse, view from east, showing Backshop and Boiler House.
City of Victoria (2003)

A.1 Introduction

Focus Equities is preparing a Master Plan and Rezoning Application for the E&N Roundhouse Historic Railway Precinct along Esquimalt Road in Victoria West, to be submitted to the City of Victoria. It follows the overall vision for the site as an animated mixed-use development that will serve both the local community and visitors to Victoria.

This History and Heritage Report comprises a background document for the rezoning application. The application updates the Master Plan that was approved by the City of Victoria in 2008. Harold Kalman, the author of the present report, participated in the 2008 planning initiative as a principal of Commonwealth Historic Resource Management. The client at the time was Roundhouse Properties Limited Partnership, the predecessor to Focus Equities.

Since that time, the roundhouse structure has been stabilized and one of the ancillary buildings has been put into active use. Several residential buildings have been constructed on the larger site, including The Promontory and the Shutters.

The current initiative by Focus Equities to redevelop Roundhouse at Bayview Place will continue to integrate new construction with upgrades to the historic buildings on the site.

The History and Heritage Report addresses the treatment of the old buildings and recommends a series of strategies to guide their rehabilitation and their integration with the new components. The report also describes the opportunities and constraints that arise from working with a group of protected heritage structures. It recognizes that the entire Roundhouse complex will be considered as both a new, sustainable development and an upgraded historic place. The overall planning approach for the Roundhouse at Bayview Place will celebrate the close relationship between new and old.



Roundhouse at Bayview Place

From Rezoning Application

A.2 The Esquimalt & Nanaimo Railway

The Esquimalt & Nanaimo Railway (E&N) was constructed in 1886-88 and subsequently expanded to provide passenger and freight service between Victoria and Courtenay (290 km), with connections to Port Alberni and the Nanaimo waterfront.

- The E&N was formerly the economic lifeline of Vancouver Island.
- Its original purpose was to transport coal from Robert Dunsmuir's mines near Nanaimo to the harbour at Esquimalt, where it was shipped to many markets.
- It operated as a subsidiary of the Canadian Pacific Railway (CPR).
- It began under steam power, with diesel power being introduced from 1948.
- The E&N developed the Roundhouse site in 1912-13 at its southern terminus in Victoria.
- As with all rail transport across Canada, the E&N gradually lost dominance to rubber-wheeled travel.
- The line was last operated under contract to the CPR by the Southern Railway of Vancouver Island.
- Passenger service on the E&N was discontinued in 2011.
- At present there are no train movements on the E&N line.
- Resumption of service to Courtenay is not currently under consideration.
- Proposed rail commuter service between Langford and Victoria is being discussed and, if implemented, the line might use the southern portion of the E&N right-of-way.



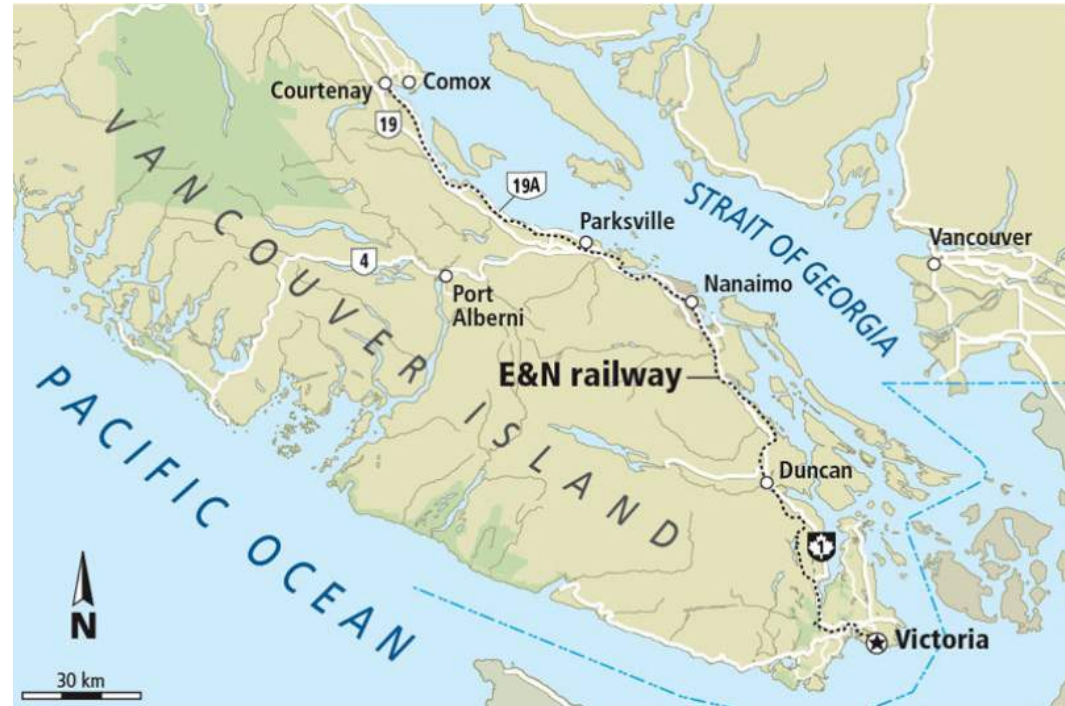
The E&N Dayliner

The E&N Dayliner operated Budd self-propelled diesel equipment.

The Island Corridor Foundation

The assets of the E&N Railway are owned by the Island Corridor Foundation (ICF).

- The ICF is a non-profit society and federally registered charity established in 2006 for the purposes of owning and managing the rail corridor on Vancouver Island.
- The Board of Directors represents local governments and First Nations
- Its principal objective is to facilitate the rehabilitation of the rail corridor, to support economic, social, and cultural development.



Existing E&N Rail Corridor

A.3 Roundhouse at Bayview Place

Site and History

The Roundhouse site is located along Esquimalt Road in Victoria West, a short distance west of the Johnson Street Bridge and downtown Victoria.

- The site occupies 3.74 hectares (9.23 acres).
 - It is bounded by Esquimalt Road to the north, Catherine Street to the west, Kimta Road to the south, and Sitkum Road to the east.
 - A development plan for both this site and the adjacent Bayview Place Hillside, located to the southeast, was approved by the City of Victoria in 2008.
 - Focus Equities has built the Promontory and The Shutters residential developments since that time.
 - The land east of Sitkum Road, which includes The Shutters site, was formerly a Songhees village, initially settled in 1844.
 - The present planning and rezoning initiative updates and expands the approved 2008 development plan.
 - Focus Equities owns the property and intends to develop it as a mixed-use community, featuring residential and hotel uses.
 - The property is being marketed as 'Roundhouse at Bayview Place, a KW Mariash Sr. Master Plan Community.'
 - The Roundhouse complex was constructed in 1912-13.
 - It was the primary servicing facility for the E&N, which operated about 20 steam locomotives.
 - The site plan and design of the structures were produced by the parent company, the CPR, which bought the E&N in 1905.
 - The Crown sold the land, a portion of the old Songhees Reserve, to the CPR in 1911.
- The adjacent station stop was known to the CPR as "Russell's".
 - The CPR called the building the Russell's Roundhouse.
 - The strengths of the Roundhouse site for future development include its location close to the Inner Harbour and Victoria's downtown core, and its considerable frontage on a primary east-west traffic corridor.
 - The site and the larger precinct are considered to be underdeveloped. They lie in the path of Victoria's natural expansion into Victoria West.
 - The half-dozen historic structures are unused or underused. They are ready for rehabilitation and repurposing.



Aerial Photo of Roundhouse at Bayview Place Site

Archaeology

Many historic artifacts dating from the second half of the nineteenth century have been recovered over the years from this general area. An old Songhees village from the prehistoric period (i.e., pre-1846) was located southeast of the Roundhouse, on the site of The Shutters development. The Haida and Tsimshian traders who came here from the North to trade with the Songhees, and camped in the area around the present Sitkum Road, also left remains.

A professional archaeological investigation of the Shutters site was undertaken in the late 1980s. The excavation was done by I.R. Wilson Consultants Ltd. of Victoria, now a part of Stantec Inc. The work was directed by Shane Bond. The investigation was mandated under the terms of the Heritage Conservation Act, following the discovery of a single artifact that was thought to predate 1844. It is now considered to be a post-1846 artifact made to a traditional design.

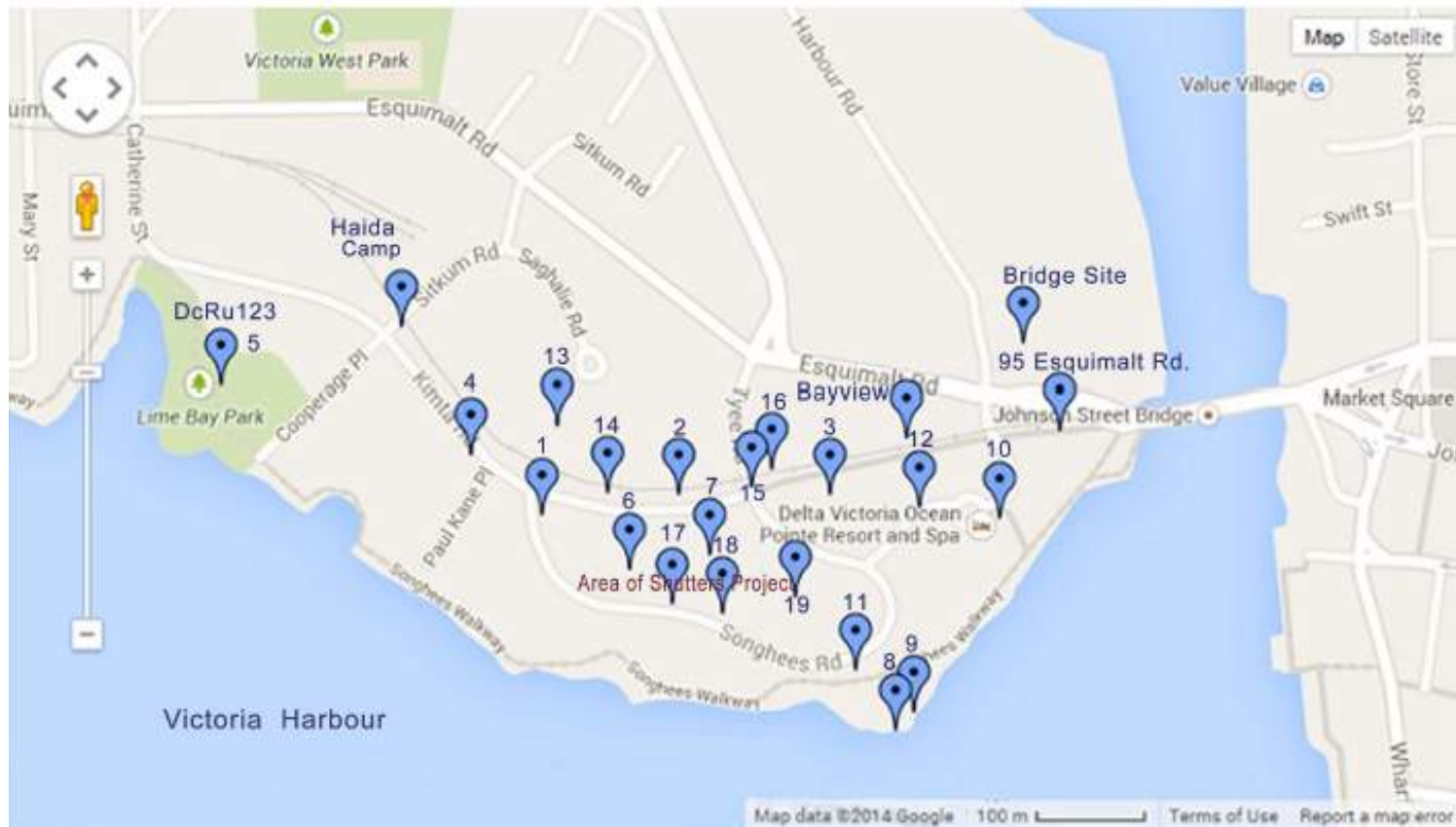
Bond's excavation recovered some 6,000 artifacts, representing both the Songhees villagers and visiting traders. These were estimated to comprise only a small fraction of the archaeological potential of the village site. The Songhees village did not extend to the Roundhouse. No pre-1846 artifacts have been documented west of Sitkum Road, other than from one collection site in Lime Bay Park.

Since no pre-1846 artifacts have been identified on the Roundhouse site, there is no legal necessity to undertake an archaeological investigation. The Government of BC does not require any risk-based investigations unless there is a demonstrated conflict between a proposed development and a known archaeological site. None exists here. The only part of the proposed Focus Equities development that may encroach on a known archaeological site is the eastern part of the seniors' housing. It appears to overlap the far western edge of the old Songhees village, settled in 1844.

Grant Keddie, Curator of Archaeology at the Royal British Columbia Museum, considers it highly unlikely that prehistoric remains would be found behind Mud Bay, which includes the Roundhouse site. He notes that bedrock lies close to the surface, and that much of the site was levelled with dynamite prior to construction of the railway and Esquimalt Road. Shane Bond of Stantec agrees, and suggests that there is a likelihood that prehistoric shell middens could be found along the former shoreline at either side of Lime Bay Park, which was formerly a burial ground, but this is located at a distance from the proposed development.

Construction on the Roundhouse site may proceed without a permit from the Archaeology Branch. An archaeological investigation might be regarded as good practice, but it is not required and would be voluntary. Should any evidence of human use that pre-dates 1846 be found during construction, all work must stop and the find must be reported. The find would likely lead to the requirement for an Archaeological Impact Assessment ('AIA'). An AIA includes deskwork and selected excavations done by shovel and/or machine. An AIA typically takes around four months and may cost around \$30-50,000. During this time First Nations are given the opportunity to review and comment to the application.

The intangible cultural heritage of the Roundhouse site is not considered in this report, other than its past railway use.



Archaeological Collection Sites

The map shows the archaeological collection sites in the vicinity of the old Songhees Village. The Roundhouse site is at the left of the map, along the railway tracks shown between Sitkum Road and Catherine Street. (Tom Brown, The Midden)

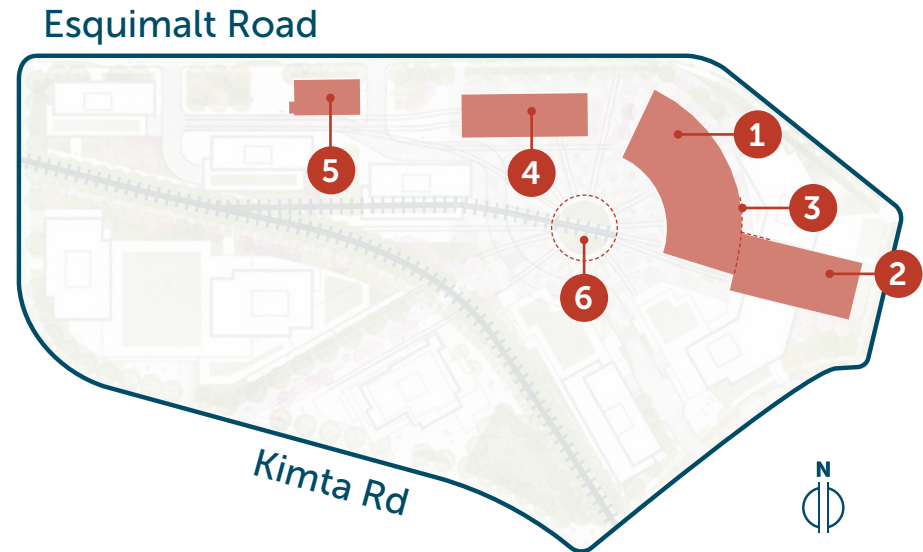
Buildings

Six structures within the Roundhouse site are considered individual heritage assets and are protected by municipal heritage designation (see Chapter 4):

- 1 Roundhouse
- 2 Back Shop (attached to the Roundhouse)
- 3 Boiler House (attached to the Roundhouse)
- 4 Car Shop
- 5 Stores Building
- 6 Turntable

The structures and the larger site retain considerable integrity. They have experienced only minor alterations over the past century.

- The buildings all have concrete foundations, brick walls, heavy-timber structural frames, and wood roofs.
- Their design is attributed to R.A. Bainbridge, the CPR division engineer in Victoria
- The contractor was E.R. Doe & Brother.
- The physical fabric experienced considerable deterioration over the years.
- Focus Equities has made a major investment in repairs and structural reinforcement since 2008. The structures are currently weatherproof and stable.





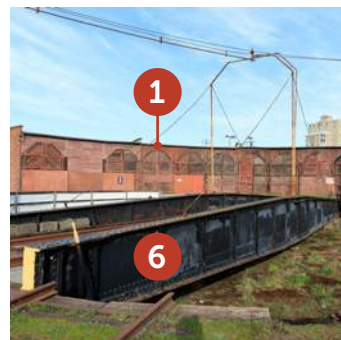
Roundhouse, view from east, with Backshop and Boiler House
Photo: City of Victoria (2003)



Car shop, view from west



Stores building, view from east



Turntable, looking north,
Roundhouse at rear

Design Guidelines

Design guidelines for the Roundhouse site were prepared in 2008 by Dialog and their associates, and revised in 2015. They will be revised for the present rezoning application.

The 2015 design guidelines include the following recommendations:

Building Form and Architectural Character

- Retain and conserve the existing buildings.
- New buildings will be one-storey, contemporary structures to contrast with the heritage buildings on the site. They should be designed with a contemporary appearance and relate to the pedestrian scale.
- New buildings are not permitted in the area between the Roundhouse and Esquimalt Road.
- New development must respect the heritage character of the historic railway buildings.
- A dynamic and animated streetscape will be created along the Esquimalt Road frontage, behind the Backshop.
- New buildings or structures south of the Car Shop will define the Turntable Plaza space and ensure that a portion of the south elevation of the Car Shop is exposed.
- New buildings are encouraged to be transparent in their materials.

Landscape Character

- The landscape should evoke feelings of an operating rail yard.
- The use of trees should be limited to the perimeter of the site, to form part of the streetscape rather than part of the precinct.
- A naturalized landscape, as is seen on parts of the existing site, is the appropriate approach in the railway precinct.

Railway Interpretation

The interpretation concept and design were addressed in the report, "Roundhouse Celebrating Our Heritage" (2008).

Previous Studies

Focus Equities and its predecessor company, Roundhouse Properties Limited Partnership, have undertaken and commissioned several studies of the E&N Roundhouse site, and have produced proposals for its future treatment. These include:

- Focus Equities, Roundhouse at Bayview Place: Vision 2020, October 2019. This document summarizes the current planning concept and principles.
- Jonathan Yardley Architect, "E&N National Historic Site, Victoria, BC: Heritage Conservation Plan", April 2012, revised January 2015.
- Commonwealth Historic Resource Management Limited, "E&N Roundhouse Site, Victoria: Heritage Conservation Report", 2 vols., February 2007.
- AldrichPears Associates, numerous studies identifying opportunities for interpretation, 2008-2018.
- Planning and feasibility studies by TownSquare Planning, ZGF Architects, Coriolis Consulting Corp., and Economic Planning Group.
- "Roundhouse Celebrating Our Heritage" 2008.

A.4 Land Use Considerations

The Roundhouse site is situated within Victoria West, a recognized neighbourhood in the City of Victoria. Its land use is regulated by several regulations and constraints:

Zoning

- The subject property is a part of the City of Victoria's CD-12 zone, 'Roundhouse District'. The Roundhouse structure is situated within Development Area 1 (DA-1) of the zone, whose area is about 148,000 square feet.
- Details of the specific zoning requirements and the scheduled allowable uses are provided by the project planner in a separate report.
- The present development initiative will include an application for rezoning the site of the Roundhouse at Bayview Place, i.e. the west half of the 20-acre property.

Neighbourhood Planning

- The City's current plans for Victoria West are found in the draft Neighbourhood Plan.
- Three neighbourhood-specific plans apply to the Roundhouse site:
 1. Railyards Development Guidelines (2002)
 2. Roundhouse Design Guidelines (Dialog, 2008)
 3. Vic West Transportation Plan (2008)
- The site is also subject to the provisions of the City of Victoria Official Community Plan.

Master Development Agreement

Focus Equities and the City of Victoria have entered into a Master Development Agreement (MDA). The document defines the responsibilities of the two parties in the intended development. The MDA will be revised as a part of the present rezoning application.

Heritage Status

- The Roundhouse and its site have been designated as heritage property by the Government of Canada and the City of Victoria.
- The Province of British Columbia delegates the identification and protection of heritage property to local governments.

Government of Canada

Designation and Listing

- The Roundhouse site was designated in 1992 as a National Historic Site by the federal government, pursuant to the Historic Sites and Monuments Act.
- The official name of the designated site is "Esquimalt and Nanaimo Railway Roundhouse National Historic Site of Canada".
- The official recognition "refers to the area enclosing the 10-stall roundhouse building with attached machine shop ...and the detached turntable in its circular pit" as well as the "approach tracks" and "the space between the main building and the turntable".
- It is praised as "one of the finest, and most intact examples of industrial heritage railway architecture in British Columbia".

The Roundhouse is listed in the Canadian Register of Historic Places as “E&N Roundhouse”.

- The site is defined as “a collection of brick and wood industrial buildings, and a locomotive turntable, on Esquimalt Road in Victoria West”.
- The Statement of Significance (‘SOS’) describes the historic place, its heritage value, and its character-defining elements. It is contained in Appendix 1.

Effects of Federal Heritage Listing and Designation

- Listing on the Canadian Register of Historic Places is done for identification and information purposes only. Listing confers no benefits or constraints.
- Federal designation as a National Historic Site is accompanied by no requirements for protection. The designation is an honorific, not a constraint.
- Federal designation as a National Historic Site makes the historic place eligible for financial assistance under the National Cost-Sharing Program.
- A small amount of funding is available: \$1,000,000 nationally for 2020-21.
- The intake for applications closed on November 15, 2019. No further funding has been announced.
- Nevertheless, it would be prudent to apply for financial assistance under the program, in order to secure a place in the queue should the program resume in the future.

Federal designation makes the site eligible for a plaque. The approved plaque text is:

“This imposing brick roundhouse is a particularly fine example of an industrial structure associated with the steam railway era in Canada. Built in 1913, the ten-stall roundhouse with attached machine, boiler and blacksmith’s shops is part of a remarkably well-preserved railway yard that also includes a car shop and stores building. The roundhouse complex served the Esquimalt and Nanaimo Railway long after the introduction of the diesel locomotive in 1949 and the railway’s operational relocation to Nanaimo in the 1950s. This site is an important reminder of Canada’s rich railway heritage. Approved 1999”.

City of Victoria

Heritage Designation

The Roundhouse and two ancillary buildings (the car shop and the stores building on Esquimalt Road) have been designated as heritage properties by the City of Victoria, pursuant to the Heritage Conservation Act [RSBC 1996] Chapter 187.

- The original designation was made in 1991, under the predecessor legislation.
- Municipal heritage designation protects the historic structures from demolition and places constraints on future alterations and development. The overall character and the principal features (“character-defining elements”) of a designated building or place may not be changed without the formal approval of City Council. The Council is advised in this respect by the Heritage Advisory Panel.
- The character-defining elements are identified in the Statement of Significance, reproduced here as Appendix 1.

Heritage-designated buildings are subject to the following conditions:

- A designated building can be sold or bought without special approval.
- The owner retains all rights to the individual enjoyment of his property.
- Activities in the building or on the property are not affected by designation.
- Normal maintenance and repairs (e.g. painting, gutters, maintenance of stairs) can be carried out at the owner's sole discretion.
- An owner should consult the Standards and Guidelines for the Conservation of Historic Places in Canada (Government of Canada, 2010) to determine what changes are acceptable and the appropriate design approach.
- A designated building cannot be demolished without the consent of City Council.
- Alterations or additions needing a building permit and affecting the designated portions of the building cannot be made without the consent of City Council, which in effect means that they require a Heritage Alteration Permit (HAP).

Heritage Revitalization Agreement

The Roundhouse site is subject to a Heritage Revitalization Agreement (HRA) with the City of Victoria, pursuant to Section 966 of the Local Government Act. The HRA obligates the owners to preserve the heritage structures and to undertake work in accordance with the Heritage Conservation Plan prepared by Jonathan Yardley, with reference to Commonwealth's Heritage Conservation Report (2007). The work is to be overseen by Yardley (who is since deceased) or by another registered architect and professional heritage consultant, and 'in accordance with good engineering and heritage conservation practices and otherwise in accordance with the Standards and Guidelines for the Conservation of Historic Places in Canada (2010).'

A.5 Recommended Strategies and Actions

Archaeology

- There is no formal requirement to undertake further archaeological investigation, nor for an archaeological impact assessment. Doing so might be regarded as good practice, but in this situation it is voluntary.
- If any evidence of human use predating 1846 is found during construction, work must stop and the find reported to the Archaeology Branch. The find would likely lead to the requirement for an Archaeological Impact Assessment (“AIA”).
- Since the present work is an extension of the development plan approved by the City of Victoria in 2008, no further consultation with First Nations is legally required.
- Construction on the Roundhouse site may proceed without the formal requirement for a permit from the Archaeology Branch.

Buildings, Structures, and Landscapes

- All work to existing buildings, structures, and landscapes will conform to the Standards and Guidelines for the Conservation of Historic Places in Canada (Parks Canada, 2010; see Appendix 2). This has been adopted as a guidance document by the City of Victoria under the Official Community Plan.
- The design of new buildings will follow, where appropriate, Dialog et al, Roundhouse Design Guidelines (2008, rev. 2015).
- New buildings on the core site should respect, without imitation, the dominant forms of the existing buildings. Most buildings (other than the Roundhouse itself) are rectangular in shape and covered with medium-sloped gable roofs.
- New buildings on the core site should respect, but not necessarily imitate, the dominant materials of the existing buildings. These include brick walls, heavy-timber structures, and wood doors and windows.

- New buildings and landscapes should respect and continue the site’s dominant industrial character.
- These recommendations apply to initiatives at both the macro level (e.g., new buildings) and the micro-level (e.g., street furniture).
- New construction should engage and establish a dialogue with the existing structures. This approach is expressed in the Design Principles articulated in Roundhouse at Bayview Place: Vision 2020: “engage, integrate, and activate heritage”.
- The effect of interventions to the site should be to “Make it Sparkle!”

Sources

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Richard Linzey, BC Heritage Branch

Appendix 1: Statement of Significance

(from the listing on Canada's Historic Places, the Canadian Register of Historic Places)

- https://www.pc.gc.ca/apps/dfhd/page_nhs_eng.aspx?id=97

Esquimalt and Nanaimo Railway Roundhouse National Historic Site of Canada

Description of Historic Place

Designed in 1912 and completed in October 1913, the Esquimalt and Nanaimo Railway Roundhouse is located in Victoria, British Columbia. The site is comprised of three sections: the locomotive roundhouse and two attached structures that housed various shop facilities for the servicing of steam locomotives. The roundhouse building was built to a segmental- arced plan and features a curved front and rear wall constructed of solid red brick. It contains ten stalls that face onto an operational turntable with an 85-foot diameter. The machine shop is a rectangular brick building that abuts the rear southeast corner of the roundhouse. The roundhouse complex is surrounded by associated site features, notably the turntable, sidings and tracks, and two freestanding brick buildings which are components of the industrial complex and are contemporary to the roundhouse. The official recognition refers to the area enclosing the 10-stall roundhouse building with attached machine shop, both on their footprints, and the detached turntable in its circular pit. This area also includes approach tracks. The space between the main building and the turntable is also part of the designated place.

Heritage Value

- The Esquimalt and Nanaimo Railway roundhouse was designated a national historic site of Canada in 1992 because it survives virtually untouched since its construction in 1912. Surrounded by various well-preserved related shops and railway outbuildings, it is a particularly fine example of an industrial structure associated with the steam

railway era in Canada.

- The Esquimalt and Nanaimo roundhouse is located on the site of the original 1886 railway terminus for Victoria. This industrial complex was built by the Canadian Pacific Railway to serve as the primary servicing facility for the steam locomotives and rolling stock of the Esquimalt and Nanaimo Railway. The facility served this purpose until 1949 when steam locomotives were replaced by diesels on the island. After that date the roundhouse served as a service facility for the E & N's diesels, and currently services the passenger dayliners used by the railway. Remarkably, the roundhouse and contemporary shops have survived with minimal alterations since the time of their construction.
- The roundhouse complex, consisting of the roundhouse itself, the attached machine shed, the roundtable and the approach tracks, conveys a clear sense of its functional purpose in its structure and organizational layout. The roundhouse itself curves around the turntable and features ten large openings creating ten work bays for the repair of the locomotives. The large windows at the rear of the building provided natural light into each of the 10 work bays. The machine shop is attached to the rear of the roundhouse and cars could be moved into the building through the roundhouse or by a separate track leading off the turntable. The machine shop is divided internally into two sections by a brick partition wall. The rear area was built to house a blacksmith shop and boiler shop.
- A distinct sense of place is conveyed by the uniformity of materials, fenestration, building configuration, and by presence of the turntable that links the entire complex of buildings into an efficient industrial plant. The Esquimalt and Nanaimo roundhouse complex is the most intact facility associated with the servicing of steam locomotives in western Canada.
- Source: The Historic Sites and Monuments Board of Canada, Minute, February 1992.

Character-Defining Elements

The key elements relating to the heritage value of this site include:

- The site in the Songhees area of Victoria West, across the Inner Harbour from downtown Victoria;
- The historical relationship between the roundhouse facility and Victoria as embodied in the track corridor linking the site to the downtown terminus via the Johnson Street bridge;
- The site in its defined boundaries and its continuous association with the Esquimalt and Nanaimo Railway from the railway's inception in 1886 to the present day;
- The form, massing and exterior design features of the roundhouse including the segmental floor plan, spatial organization in relation to the turntable, solid brick construction, large windows and door placements and surrounds;
- The large open bays of the roundhouse with their the original wooden doors and associated hardware and surround details;
- Interior structural elements of the roundhouse including the roof truss system, timber supports, and original wall surfaces;
- Roof details and mechanical elements including smoke jacks, ventilators and skylights;
- The form massing and exterior design features of the machine shop including the rectangular plan divided into two areas separated by a brick wall, large segmental windows, round- headed locomotive entrance, engaged brick piers, and gabled roof;
- Railway tracks within the buildings, on the turntable and leading up to the building;
- Vestiges of industrial equipment including the floor pits and aprons, the steel turntable, concrete-walled turntable pit, and associated tracks and machinery.

Appendix 2: Standards and Guidelines

This appendix references relevant excerpts from the Standards and Guidelines for the Conservation of Historic Places in Canada. The guidelines excerpted below are those that are relevant to the conservation of the six historic buildings. The full document also includes guidelines that pertain to archaeological sites and land use. The full Standards and Guidelines are available at <https://www.historicplaces.ca › media › 81468-parks-s+g-eng-web2>.

Relevant portions:

1. STANDARDS

2. GUIDELINES

- 4.3 Guidelines for Buildings

- 4.3.3 Roofs

- 4.3.4 Exterior Walls

- 4.3.5 Windows, Doors and Storefronts

- 4.3.8 Structural Systems

- 4.5.1 All Materials

- 4.5.2 Wood and Wood Products

- 4.5.3 Masonry

- 4.5.4 Concrete

- 4.5.6 Glass and Glass Products

