



HarrisGreen
VILLAGE

StarlightTM
Developments

draft

HARRIS GREEN VILLAGE

URBAN DESIGN MANUAL

JANUARY 30, 2020



DEVELOPER:

STARLIGHT DEVELOPMENTS

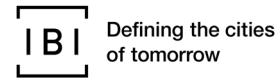


Starlight Developments and Starlight Investments hold a North American portfolio of multi-residential and commercial properties. Our Canadian properties are located from coast to coast, with more than 35 multi-residential buildings in British Columbia. We align ourselves with professional management partners and share a common goal of building and operating best in class communities for our residents.

Since our first local investment in 2011, we have grown to become one of the largest rental housing participants in Greater Victoria. With the support of local property management firms, we have expanded our Victoria real estate portfolio to include a dozen buildings with approximately 850 suites, and approximately 250,000 ft² of commercial space.

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HAPA
 COLLABORATIVE

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HEROLD
 ENGINEERING

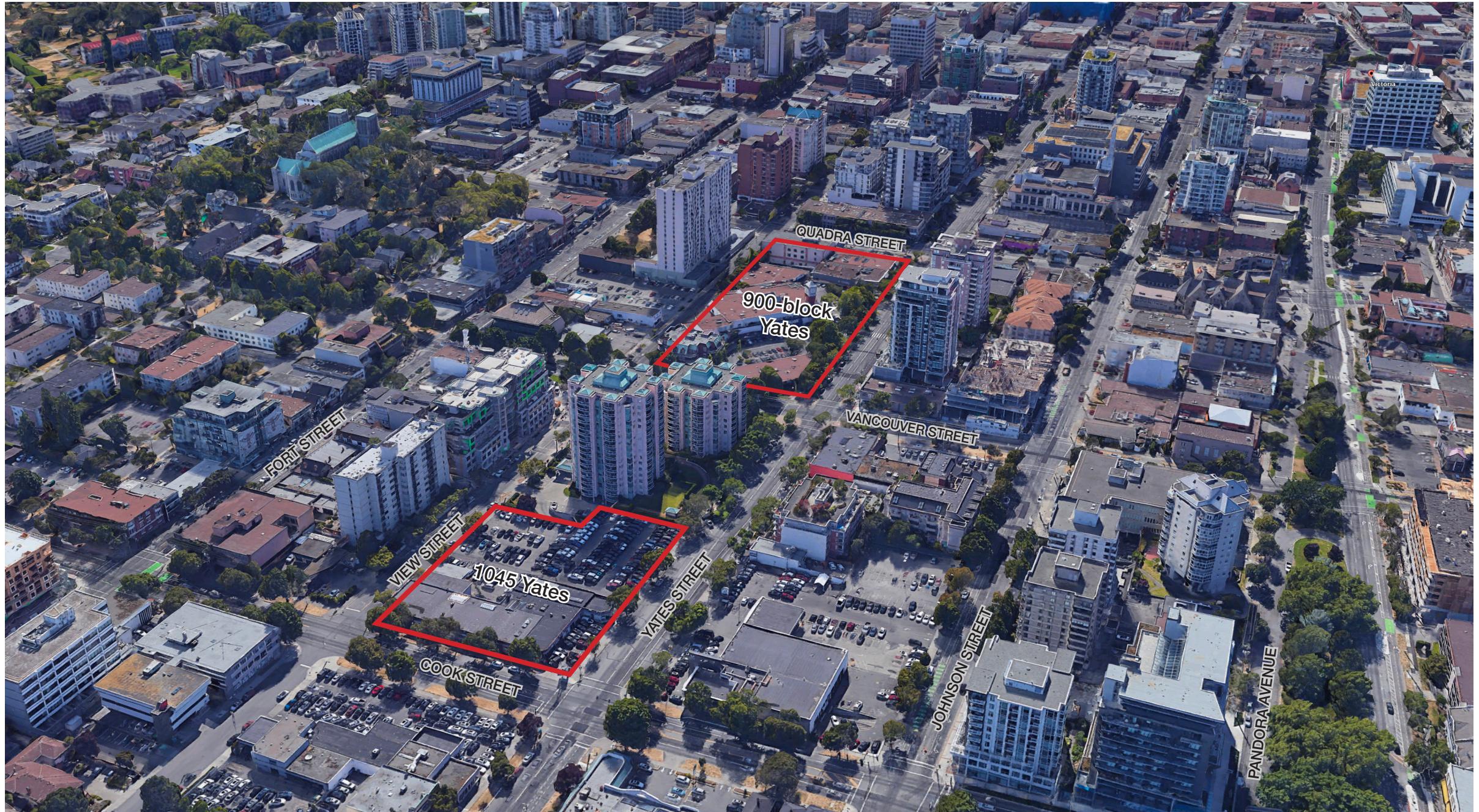
TRANSPORTATION: **WATT CONSULTANTS**

NOTE TO USER

This document has been formatted for double sided printing on legal paper (14 x 8.5; 216 x 356mm)

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1 | Introduction

DEVELOPMENT OVERVIEW

The Harris Green Village (HGV) Urban Design Manual (UDM) forms part of a rezoning application for the Harris Green Village project. The project consists of redevelopment of two properties on the south side of Yates Street between Quadra and Cook. They are comprised of the full 900-block and the eastern half of the 1000 block (1045 Yates).

The purpose of the rezoning application is to allow a mix of uses and building that will form a vibrant urban development that complements the character of the surrounding Harris Green neighbourhood. The long-term redevelopment will be subject to the new zone category as well as the design guidelines and other parameters documented in this Urban Design Manual.

Among other amenities, the central focus and signature element will be a public open space comprised of a plaza, an amphitheatre-like terraced incline and the View Street Green, a neighbourhood scale green space.

The proposed program of uses over the two sites includes:

- A publicly accessible plaza
- A publicly accessible green and connecting terraces
- Richly appointed streetscapes
- A Floor Space Ratio (FSR) of 6.0:1
- Approximately 1500 apartments ranging from studios to 3-bedrooms (rental)
- Approximately 9% of the floor area of the complex will be dedicated to commercial retail, offices and daycare uses (leased)

SUBJECT PROPERTIES

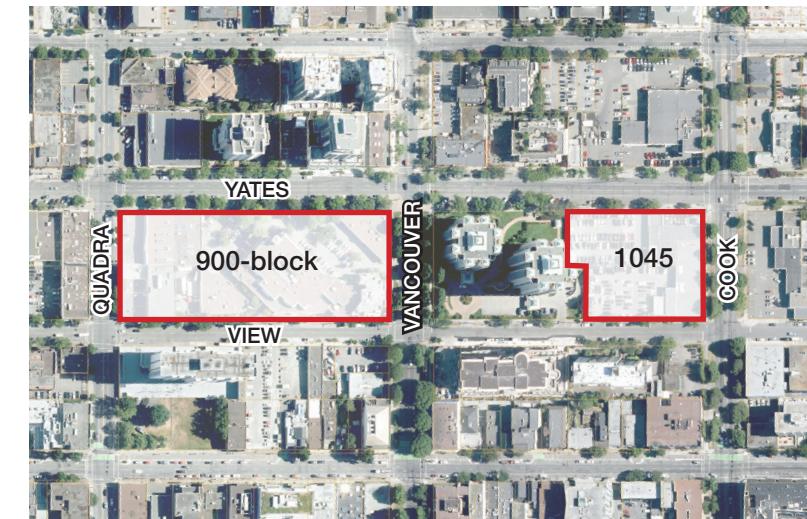
While aspects of the guidelines may be transferrable to other places, they have been crafted specifically for the 900-block of Yates Street and eastern half of the 1000 block of Yates Street (1045 Yates).

PURPOSE OF THIS DOCUMENT

The purpose of the HGV Urban Design Manual is to provide quantitative parameters and qualitative guidelines in support of the proposed site specific Harris Green Village zoning criteria. The guidelines are supplementary to and customizations of those intended by The Downtown Core Area Plan (DCAP) dated September 2011. The UDM also proposes site specific refinements to certain strategies included in the DCAP.

The stated Urban Design Objectives from the DCAP, 2011 are as follows:

1. The natural setting of the city is considered with development and urban design initiatives.
2. Development and urban design initiatives support economic viability, sustainability and place-making.
3. The qualities of the Downtown Core Area are enriched including its neighbourhoods and character areas by providing development that is appropriate to the building scale and its local setting.
4. The Downtown Core Area contains meaningful destinations that are connected and integrated with well-designed travel networks to encourage pedestrian activity.
5. Development and urban design initiatives are designed to address and respond to future changes in use, lifestyle, economy and demography.
6. The Downtown Core Area contains a diverse mix of building forms and public spaces.
7. The Downtown Core Area provides a blend of new infill development and rehabilitated heritage resources.



Aerial Key Plan

The stated Streetscape Objectives from the DCAP, 2011 are as follows:

1. That the Downtown Core Area contains pedestrian-friendly streetscapes that are inviting and active.
2. That streetscapes are legible, attractive and strengthen local identity.
3. That streetscape improvements provide a physical environment that supports and benefits businesses.
4. That the urban tree canopy is enhanced with tree-lined streets.
5. That public amenities and streetscape improvements are appropriate for the function and character of each area.
6. That wider sidewalks are provided where possible.

At the time of the preparation of this document (2019/20), the DCAP (2011) was in review by the City of Victoria planning department. The intention of the review is to update the DCAP to address certain challenging aspects that have emerged during implementation of the plan since its adoption. For specifics of these aspects, one can

refer to the planning department. Primary deviations from DCAP guidelines pertinent to this application relate to the built form regarding primary or secondary streetwalls, as well as building heights.

This document strives to be true to the salient intentions of the DCAP (2011) while responding to the pertinent characteristics of the specific sites and the extensive recent and anticipated future development of the surrounding context. The guidelines contained within the UDM endeavour to balance between providing the City with an appropriate architectural typology, massing and scale, while allowing enough design latitude for architectural creativity as well as flexibility to respond to changing impetuses for development and local conditions over time.

USE OF THIS DOCUMENT

The Urban Design Manual is a support document to the proposal for the redevelopment and its Rezoning and Official Community Plan amendment applications of January 30, 2020. The content of this manual is intended to provide rationale and guidelines for the design of the proposed project for the long-term redevelopment of the subject sites. The manual contains enabling urban design directives and guidelines that interpret and essentially respect the intentions of the Downtown Core Area Plan (DCAP 2011) with respect to the interface between buildings and uses on private properties, neighbouring private properties and the adjacent public realm. The principles and stated intentions contained in the HGV Urban Design Manual will be the overarching guide to development of the subject lands.

The individual guidelines and design strategies contained herein are not exclusive nor exhaustive. Throughout the long-term, phased development and the prescribed approval processes that will apply, new design strategies may emerge or refinements to the guidelines necessitated. This means that interpretation and adaptation of the concepts and guidelines contained in the manual will remain important determinants for the work of the Architects and designers of the development proponent team(s) and the city planners who will administer them. It is also anticipated that substantial deviations from the recommendations and guidelines will require careful consideration and substantive urban design rationale to be presented by the proposing design team(s) and accepted by city officials. The need for flexibility is acknowledged and in time, the manual may require both review and updating.



CIVIC REGULATION

The following City of Victoria policy documents are intended to be used in conjunction with the Harris Green Village Urban Design Manual wholly or in part:

City Of Victoria Official Community Plan

The City of Victoria's Official Community Plan (OCP) was updated in April 2019. It provides a general framework of objectives and policies to inform decisions on land management and planning.

It is anticipated that an OCP amendment will be required to permit the rezoning to be granted.

Downtown Core Area Plan (DCAP)

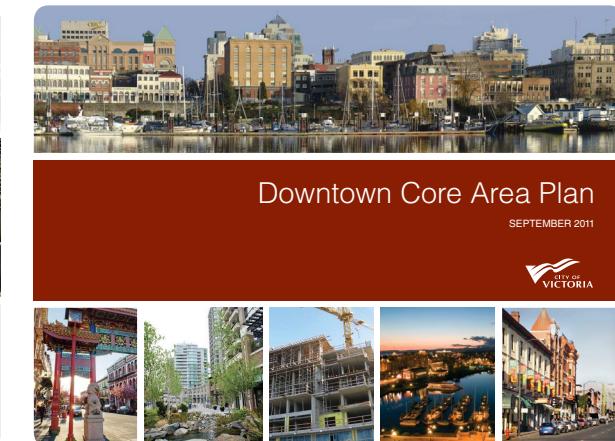
The Downtown Core Area Plan, 2011 provides land use, physical development, transportation and mobility, vitality and sustainability policies and actions for the neighbourhoods that fall within the Downtown Core Area.

It is anticipated that the Guidelines and other contents of the Harris Green Urban Design Manual will replace some aspects of the DCAP requirements.



Official Community Plan

JULY 2012
Updated: April 17, 2019

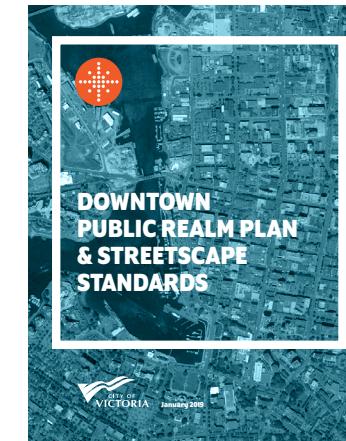


Downtown Core Area Plan

SEPTEMBER 2011

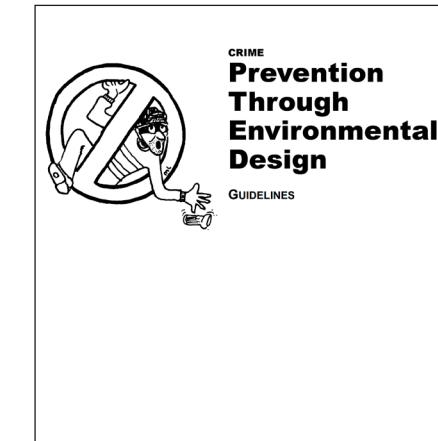


DOWNTOWN PUBLIC REALM PLAN & STREETSCAPE STANDARDS



CRIME Prevention Through Environmental Design

GUIDELINES



draft

2 | Development Concept

VISION STATEMENT

The overall vision for both sites has been shaped by the proponents' depth and experience as an owner and operator of high quality purpose-built rental mixed-use communities in urban settings, the site's surrounding context, the City's policies and the community's input.

These factors have resulted in a vision for Harris Green Village as a community with a wide range of uses and a dynamic mix of activities that celebrates the diversity of the neighbourhood and reinforces its role as a vibrant community gathering place.

The project will feature rental residential housing, street level shops and services, office space and a daycare space. The public realm is of primary importance to this project. The spaces around and between the buildings will include a series of large and small public spaces, and buildings will be arranged to reinforce these spaces and the streets with activity and allow for excellent sunlight exposure. At the centre of the 900-block Yates, there will be a large, publicly accessible open space with a variety of characteristics and activities, ranging from lively street-facing civic plaza to a quiet residential green. This space is envisioned to become the heart of the development and the heart of the surrounding neighbourhood.

Buildings will be configured into pedestrian-scale blocks with active commercial and residential uses at street level that enliven the enhanced sidewalks and small corner plazas that will be developed. The buildings will wrap the edges of the blocks to frame the streets and provide internal quiet, sunny courtyards for residents. Atop the perimeter blocks will be strategically placed, elegant towers that punctuate the corners of the blocks as seen from the surrounding street level and contribute to Victoria's skyline as seen from afar.

The proposed development responds to the site's strategic location and incorporates important City policy and neighborhood input by providing a wide range of uses and amenities and important community open spaces.



Rendering view from east towards downtown

NEIGHBOURHOOD CONTEXT

Along with North Park and Fairfield, Harris Green is a residential neighbourhood adjacent to downtown with its wide range of amenities and work places, some 5-10 minutes' walk to the west. It is also well connected to the surrounding neighbourhoods and amenities to the north, south, and east by a contiguous street grid. Harris Green has a well-balanced mix of uses – including residential, commercial and institutional – that encourages walking and cycling and gives the neighbourhood a local feel that is distinct from the downtown, which has a higher proportion of visitors and daytime office workers. The scale of buildings is in transition, as larger mid-rise buildings and high rise buildings have been developed in response to the neighbourhood's central location and rapid population growth in the city.

EXISTING SITE

The two sites on the 900-block and 1045 Yates Street are located on the eastern edge of the Harris Green neighbourhood. The 900-block site covers the full block bounded by Yates, Vancouver, View and Quadra streets. The site is currently anchored by a London Drugs and the Market on Yates plus a number of smaller retailers and service providers housed in a series of low buildings. There is also a rental residential building at the corner of View and Vancouver streets. Parking is provided in several surface parking lots and underground parking and loading is accessed from View Street. The 1045 Yates site covers the eastern half of the block bounded by Yates, Cook and View streets (Regent Towers covers the western half). The site is occupied by a car dealership with a small single-storey building and a surface parking lot.

ROADS

The orthogonal grid of streets in Harris Green provides excellent connectivity within and beyond the neighbourhood. The 900-block site is bounded on the north side by Yates, a Secondary Arterial street that runs one-way towards downtown to the west; on the west side by Quadra, also a Secondary Arterial street; on the south side by View, a Local street; and, on the east side by Vancouver, a Collector street. The 1045 Yates site is also bounded on the north side by Yates and on the south side by View. Its east side is bounded by Cook, an Arterial street and a designated Truck Route.

TRANSIT

The site is well served by transit, with Yates as part of the Frequent Transit network with a bus stop on the north side of the street just west of the Vancouver intersection. Quadra and Cook streets also have regular transit service.

CYCLE NETWORK

The All Ages and Abilities (AAA) bike network runs along Cook Street and Vancouver Street. On Yates, there is a conventional bike lane on the north side running one-way towards downtown to the west. This lane may be replaced in the future by a protected lane on the south side but no specific plans are in place the time of this writing. Protected one-way bike lanes are approved for each side of Vancouver, and are expected to be constructed in 2020.

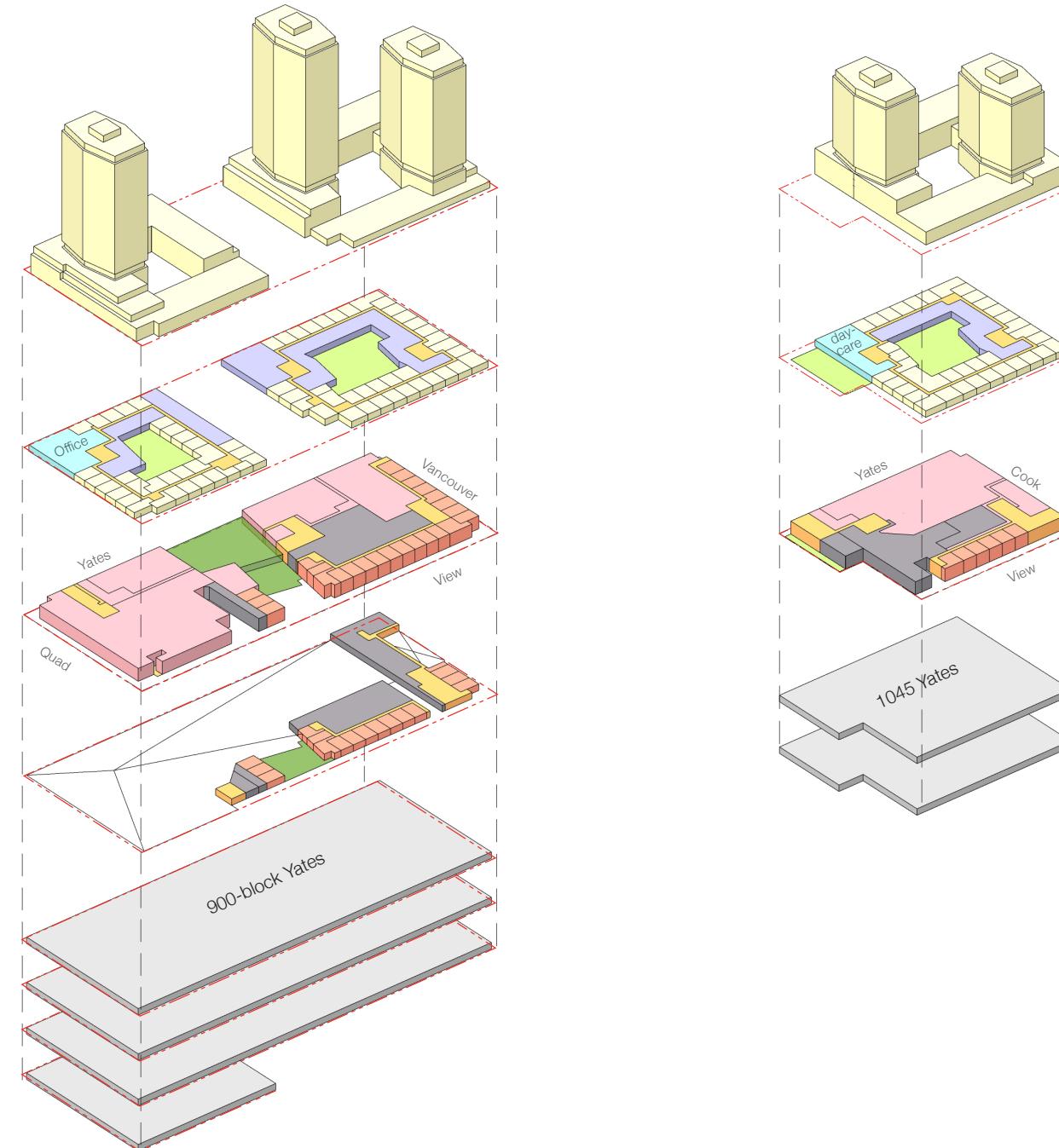
OPEN SPACE

The open space network in the vicinity of the site is modest. City parks – Pandora Green and Harris Green – are located on Pandora Street approximately 180 metres to the north, and Pioneer Square is situated about 150 metres to the south at the corner of Quadra and Meares. There are few other public spaces within a 10 minute walk of the site, and the existing sidewalk and boulevard on the 900-block's north side serves as a modest public space, with residents often socializing in front of the Market on Yates.

LAND USE

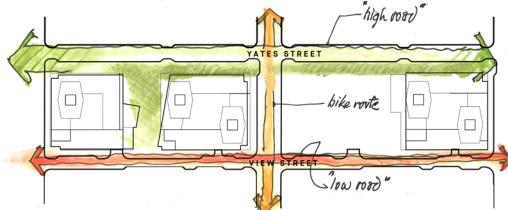
The site has three primary building uses: residential, all in rental tenure; commercial retail; and, commercial office. The residential is located above the retail, in the 4-6 level podiums, and in five towers (three on the 900-block, two on 1045 Yates). Both sites include retail shops at street level, primarily fronting Yates and the central plaza with secondary frontage on Quadra and Cook streets. The retail space will include a range of small, medium and large units that encourage a diversity of retailers and food establishments. Commercial office space will likely be located on the second to fourth levels at the corner of Yates and Quadra street.

- Commercial Retail
- Residential Lobby & Corridor
- Amenity
- Residential Rental Apartment
- Office/ Daycare
- Residential Rental Townhome
- Outdoor Amenity Area
- Open Space
- Parking
- Loading & Services



PUBLIC REALM

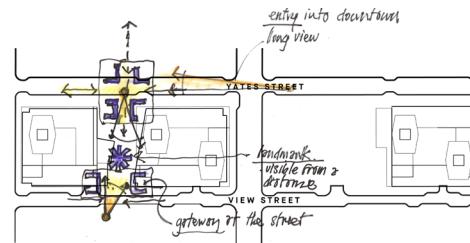
The primary contribution to the public realm is the integrated design of a significant open space on the 900-block that is intended to become the heart of the Harris Green neighbourhood. The open space consists of a lively urban plaza on Yates Street and a quieter Village Green facing View Street. These two spaces are connected by the Harris Green Terrace, an amphitheatre-like transitional space that makes the grade change universally accessible. Building setbacks provide for generous sidewalks lined with new and established street trees, benches, pet comfort stations and rain gardens. Small street corner plazas mark commercial entrances and residential lobbies on both blocks. Neighbourhood-oriented shops and services, ground level residential uses, and building forms that reinforce the pedestrian nature of the streets, add vitality to the public realm.



STREET CHARACTER

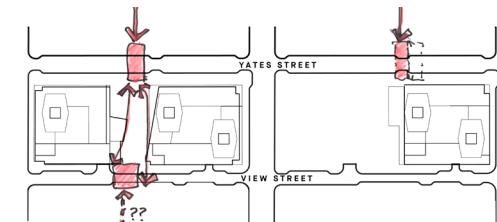
Respond to street's character with appropriate uses and building scales:

- Yates is the major street with shopping uses
- View is the minor street with residential uses



GATEWAYS

- Utilize Public Plaza as visual gateway from afar
- Create a long view into downtown on Yates Street
- Provide a landmark in the plaza that is visible from a distance
- Use the plaza to establish a gateway to the north and south extents of the site



MID-BLOCK BREAKS

- Integrate mid-block public spaces to adjacent neighbourhood
- Provide pedestrian oriented access and cues



Site Plan in the context of the neighbourhood

BUILDING MASSING AND HEIGHT (900-block)

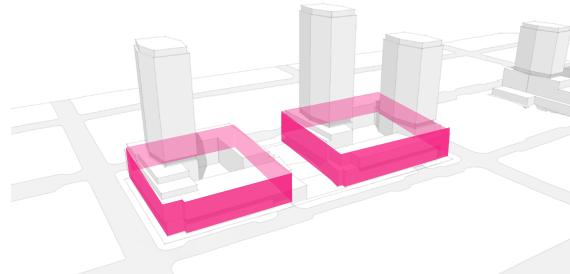
The 900-block is structured into two comfortably scaled building parcels separated by the central open space which provides a generous break in the massing of the block.

Each parcel is comprised of a perimeter podium that frames and defines the street edge and open space and that is programmed at street level with active commercial and residential uses to enliven the public spaces.

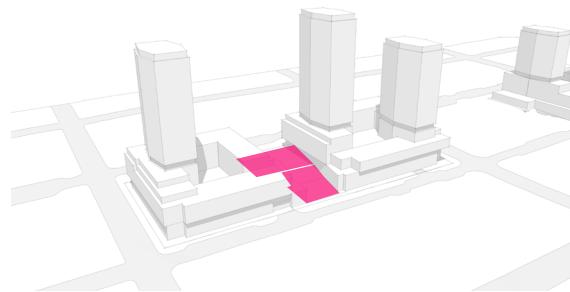
The podium's height varies in proportion to the adjacent street, being higher on the wide and busy Yates Street, lower on narrower and quieter View Street, and stepping down on the two connecting streets, Quadra and Vancouver. The podium's stepping is also carefully calibrated to maximize sunlight on the central open space and on the courtyards inside the podium buildings.

Three towers are positioned atop the podium at important corners, each set back significantly from the podium edge so its presence from the street is mitigated. The towers are positioned so they offset from one another and well separated in order to maximize views, privacy and sunlight for residents of all towers.

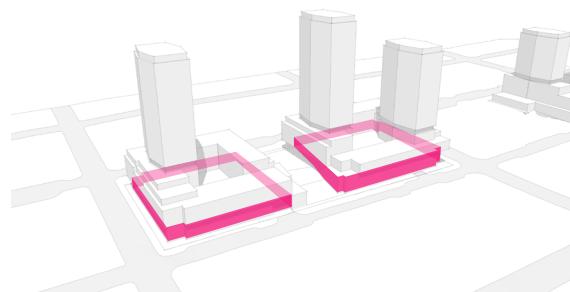
The tower heights step down towards the edges of the block from the tallest tower, which is positioned on the northeast corner of the large open space, providing a landmark for this important plaza.



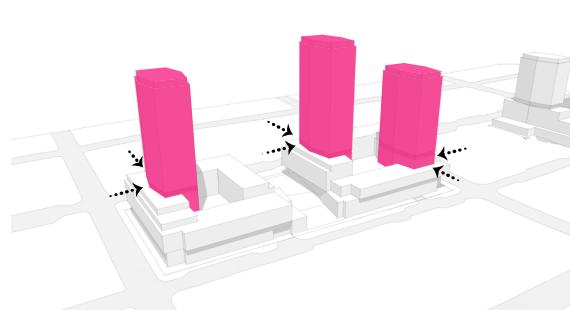
Two building parcels in perimeter block form, framing open space



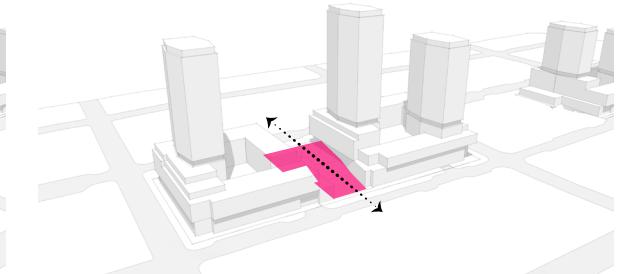
Yates Street Plaza on Yates and neighbourhood View Street Green on View



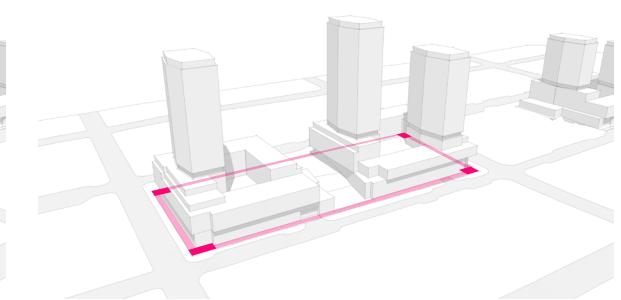
Active street edges



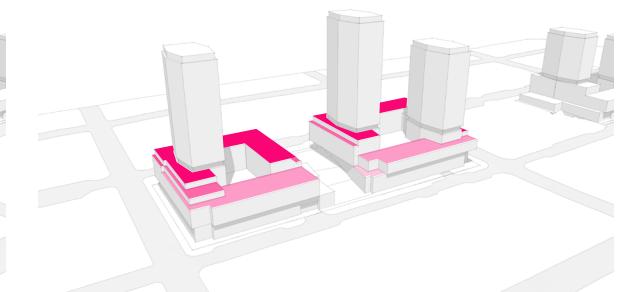
Stepped-down podiums



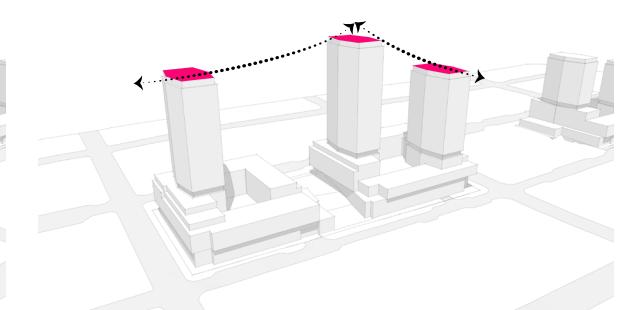
Large, central open space, with pedestrian mews connection



Small corner plazas



Stepped-down and spaced towers



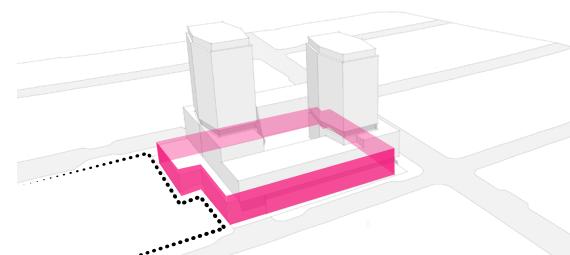
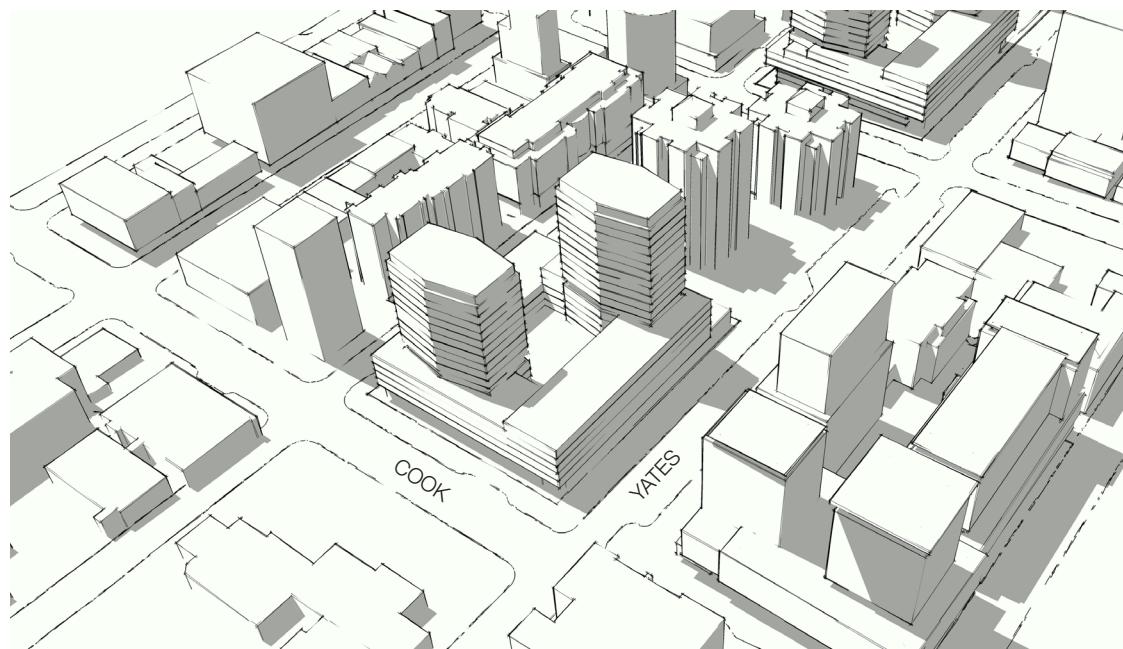
Stepped-down and spaced towers

(1045 Yates)

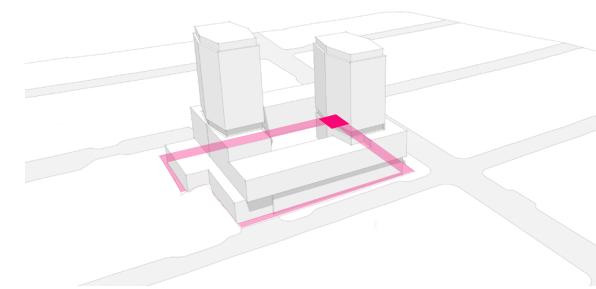
This half block is structured as a perimeter podium that frames and defines the street with active uses at street level.

The podium's height ranges from 6 storeys along Yates Street to 4 storeys along View Street, a scale consistent with the width and classification of these streets.

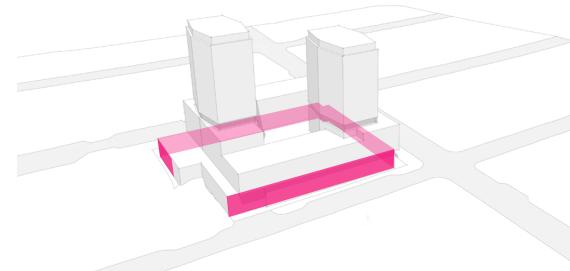
Two towers sit atop the podium with significant set back from the street edge. The towers offset relative to the adjacent Regent Towers in order to maximize views, privacy and sunlight for residents of all towers.



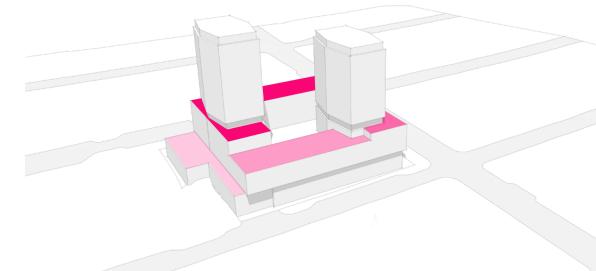
Complete the built form of the block and frame the surrounding streets



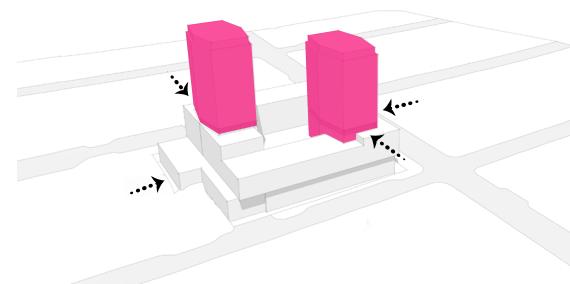
Yates / Cook gateway plaza perimeter



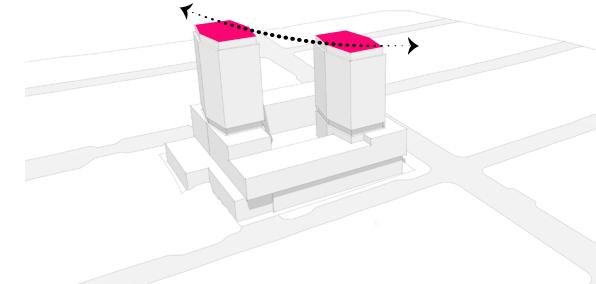
Active street edges



Stepped-down podiums



Tower positions



Stepped-down and spaced towers

3 | Urban Design

DIRECTIVES

1

New buildings to align with and define the street right-of-way and have active functions at the ground level



2

Divide the long block with public right-of-way connecting View and Yates Streets



3

Establish a new public plaza as part of a network of open spaces



4

Establish a new neighbourhood green-space as part of a network of open spaces



5

Prioritize natural universal accessibility throughout



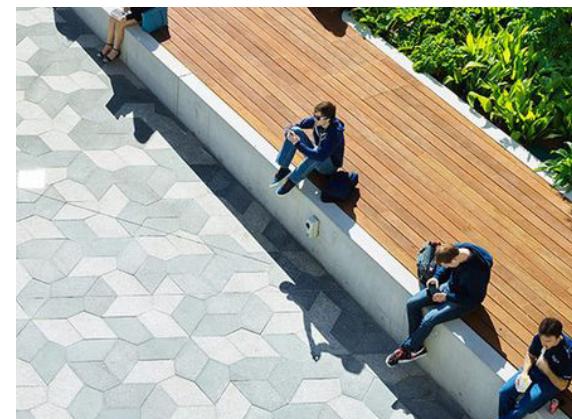
6

Make Yates Street the focus of peoples' activity, enlivened with doors and windows of retail shops and upper floor residences



7

Use high quality materials and finishes in all hard and soft landscaping, lighting and furnishing of civic and adjacent private spaces and structures



8

Use unavoidable or intentional opaque walls at street-level as elements in considered and beautiful compositions.



draft

Section 3 | Urban Design

PRINCIPLES

Connections

Incorporate the open space into a larger network, which becomes the backbone of the Town Centre



Invitation

Design the open space to be welcoming and inviting for people of all different ages, interests and backgrounds



Edge Enclosure

Utilize design elements to enclose the space making it feel contained and comfortable



Protection

Protect open spaces from vehicular traffic and noise from busy streets



Scale

Design the open space for the human scale. This requires an understanding of how people may use a space including sitting.



Comfort

Incorporate weather protection, adequate lighting and plenty of seating



Flexibility

Accommodate a wide variety of activities including large and small scale events and the every day



Delight

Once the needs of protection and comfort are met, design elements that allow for playfulness, spontaneity and delight



3.1 BUILDING COMPOSITION, ORIENTATION, FLOOR AREA & HEIGHT

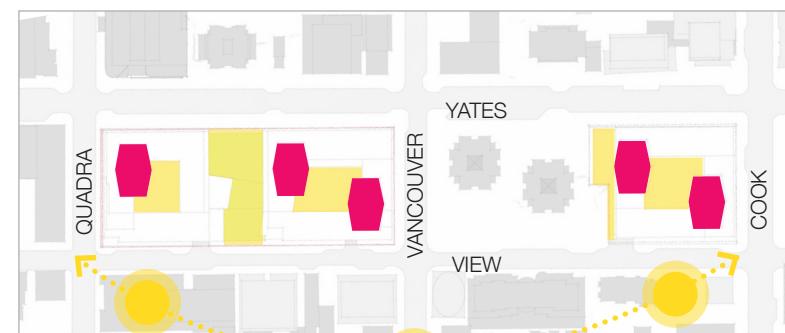
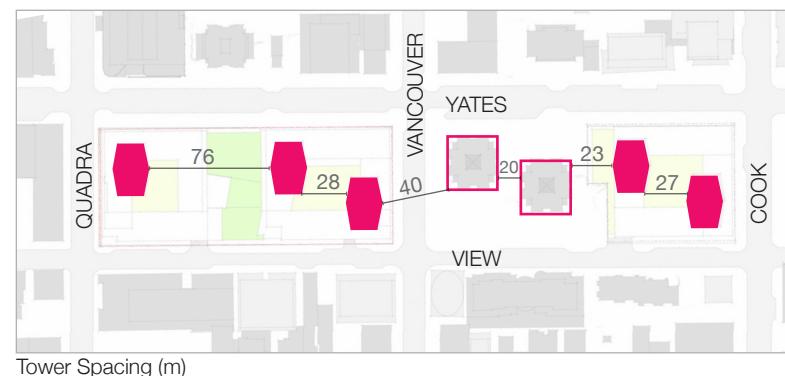
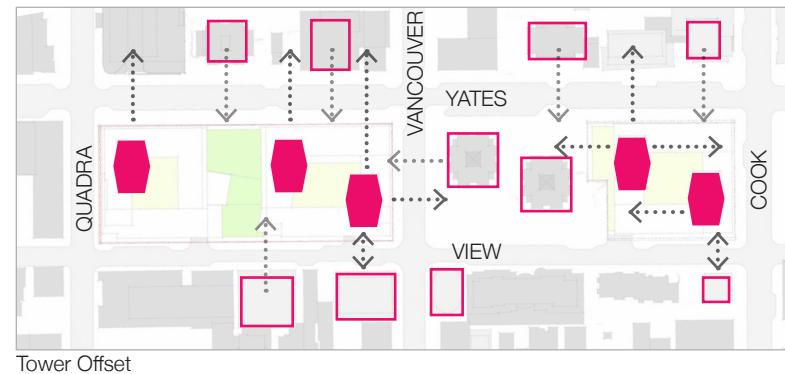
Intentions:

- To give close consideration to harmonious street-width-to-height proportions and the spacing of tall buildings.
- To achieve elegantly-proportioned, slender tall buildings rather than bulky buildings to allow views between and beyond.
- To position and shape buildings to exploit natural lighting and sky-views for people at street level.
- To consider building façades as the walls of public space and rights-of-way, and use height and alignment to create human-scaled, room-like experience versus a dominant 'canyon' effect at street level.
- To minimize shadowing and other negative impacts on surrounding neighbours, and to use architecture to contribute to a more beautiful urban environment.
- To maintain privacy of adjacent existing and future neighbours and enable views to the sky and surrounding city between tall buildings on and off the site.



Guidelines:

- a. Line the street with buildings that define and consciously shape the public right-of-way and whose use will provide vitality at the street level and views from the upper levels.
- b. At the lower floors consider the human scale, a finer pattern of materials and details as well as articulation of façade elements.
- c. Locate tall buildings to minimize shadowing of open spaces on and off-site during high-use periods.
- d. Use the low-rise (4 to 6 storeys) podium with a set back slender tower typology.
- e. Locate the tower portion with ample distance and offset from proximate tall buildings, considering the privacy, access to natural light and views of the occupants.
- f. To avoid the stepped massing (tiered wedding cake form), an interstitial floor or other architectural device, should be designed to articulate the connection and formally distinguish between podium and tower.
- g. Increase the set-backs of a portion of the ground and second levels of building corners at street intersections to expand the public sidewalk at corners.
- h. For buildings of more than 4 to 6 storeys, slender proportioned towers, are preferred over larger building floor plates.
- i. Tall buildings should be located and composed to, as much as possible, enable sunlight penetration and views to the sky from sidewalks and open spaces.
- j. Tall buildings on the same block are separated a minimum of 23m, not including architectural appurtenances such as roof and window-head overhangs, parapets, guards, handrails, decorative or artwork fin walls, slab edges, or exterior sunscreens.
- k. To achieve tower forms of more slender proportions, residential floors should not exceed 650 m² gross floor area.
- l. Consider tapered plan geometries or other architectural devices as a way to further reduce the visual bulk of buildings.
- m. Orient residential apartments to minimize the number of solely north facing windows on as many occupied spaces as possible.
- n. Where proposed underground structures are proximate to property lines shared with public rights-of-way and where these may compromise the health and survival of existing street-trees that are to be preserved, or that will impede the planting of trees in the public boulevards, it is recommended that underground structures be set-back, or appropriately altered by a method or an amount to be established by a qualified Arborist and approved by the City's Director of Parks.



3.2 FAÇADES + SETBACKS

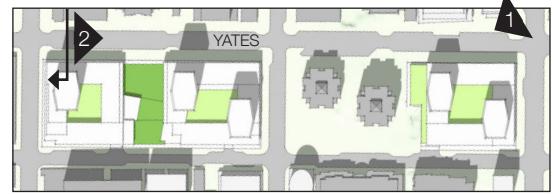
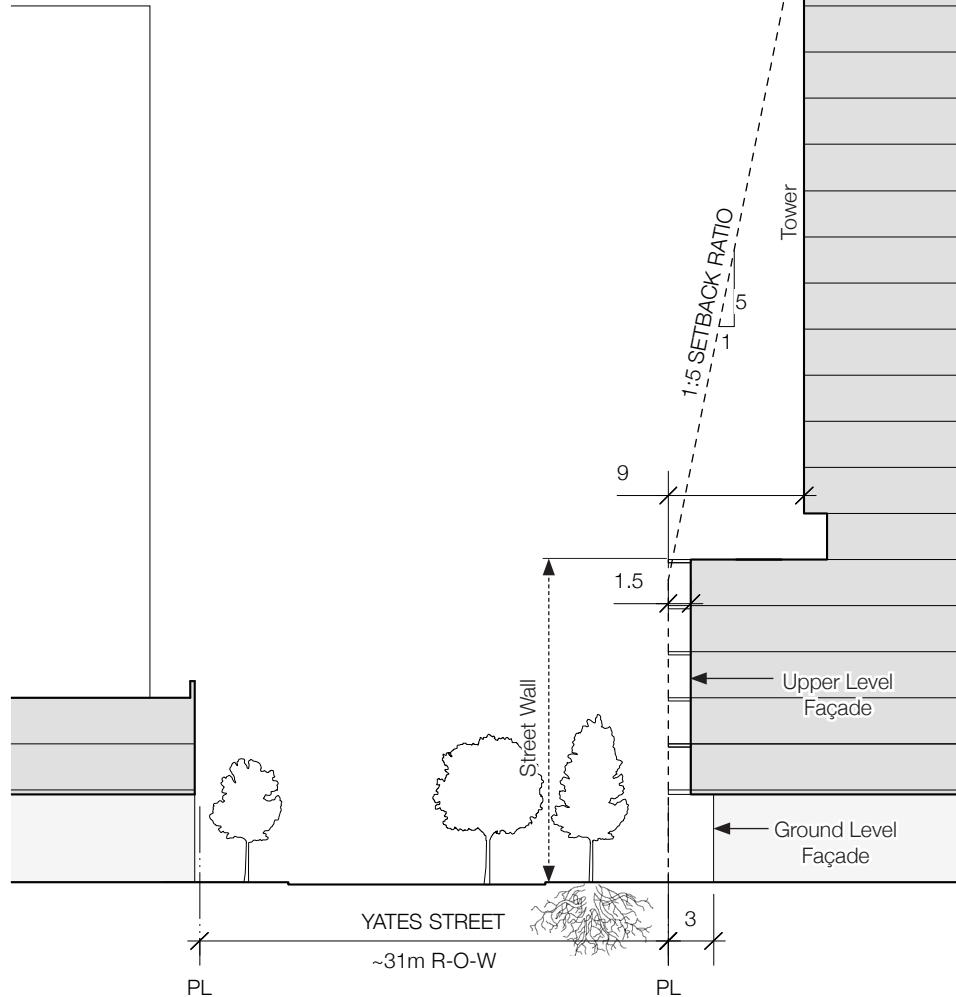
The statements of intent in section 3.1 apply to the following street right-of-way cross-sections. These drawings illustrate the location and height of the façades that delineate and define the scale of each adjacent public space and right-of-way.

3.2.1 YATES STREET

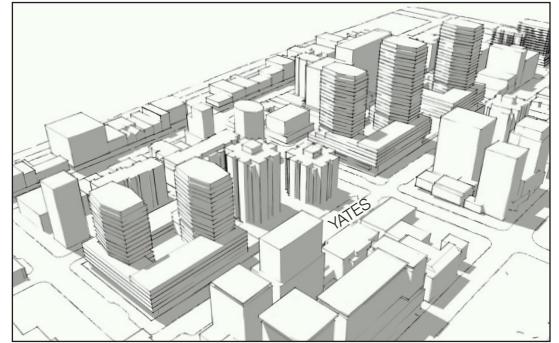
Yates Street is within a public right-of-way of approximately 31m in width. It is considered a primary commercial street per the DCAP (2011) and a major route in the public transit network. Yates runs east-west with vehicle lanes and bicycle lane traffic restricted to the west-bound direction.

Guidelines:

- a. Provide a minimum setback of 3m for the ground level façade.
- b. Provide an upper level façade along the majority of the building face located a minimum of 1.5m from the property line. This setback excludes balconies, cornices, guardrails and other architectural elements, which may project closer to the property line.
- c. The upper level façade shall be no greater than 5 storeys.
- d. The street wall shall be no less than 4 storeys.
- e. Provide a minimum setback of approximately 9m for towers.
- f. Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 20m above grade from the property line parallel to the street up to 65m above grade. No further setbacks are required above 65m.



Key Plan



1. Bird's eye view looking south west



2. Looking east along Yates Street

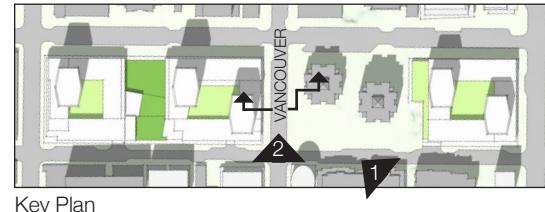
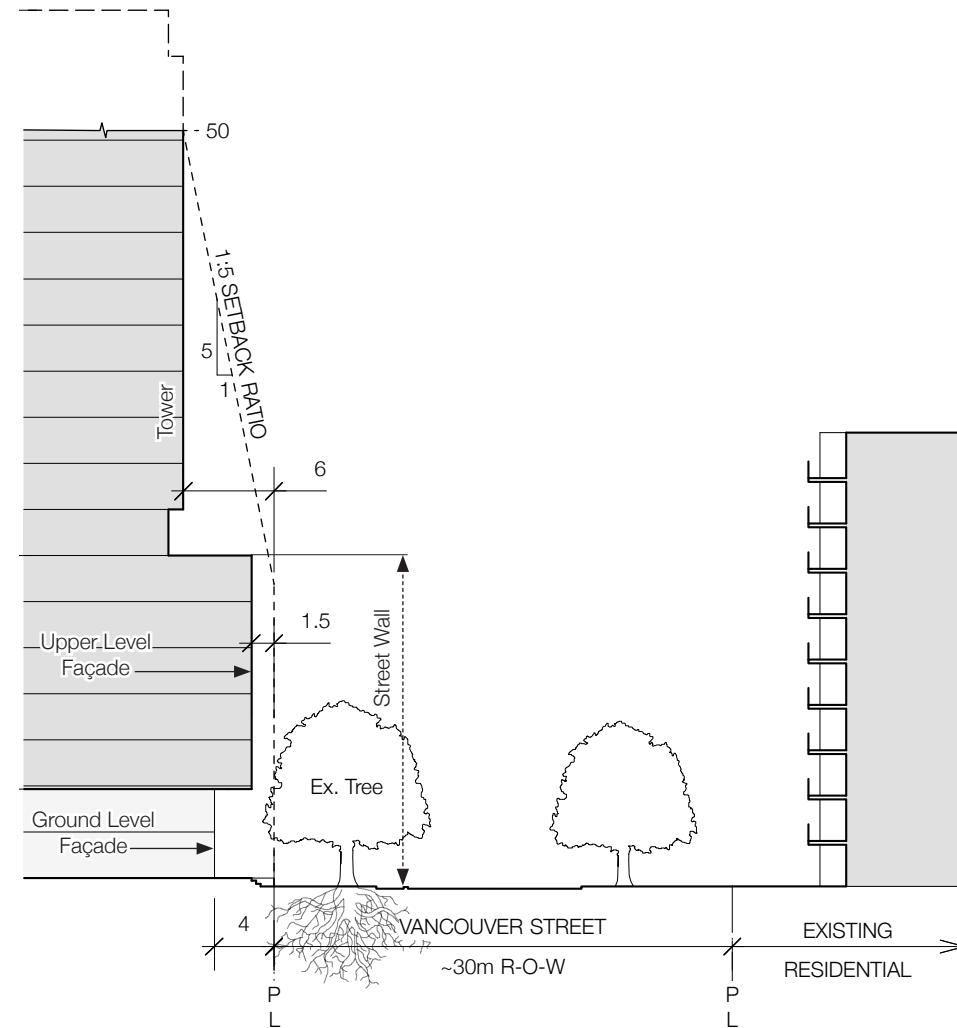


3.2.2 VANCOUVER STREET

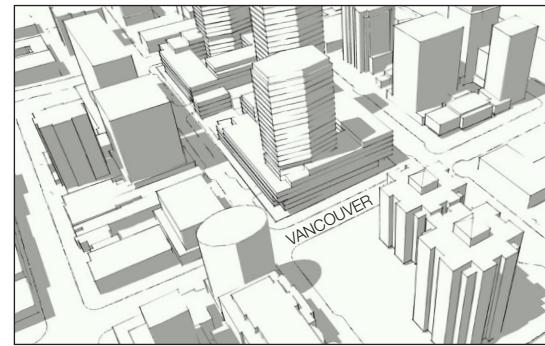
Vancouver Street is within a public right-of-way of approximately 30m in width. It is considered an 'Avenue' per the DCAP (2011). Vancouver runs north-south with two-way vehicle travel allowed. It is intended for AAA Bicycle infrastructure upgrade of separated bicycle lanes in 2020. Three significant trees are located in linear boulevard in the right-of-way along the project frontage.

Guidelines:

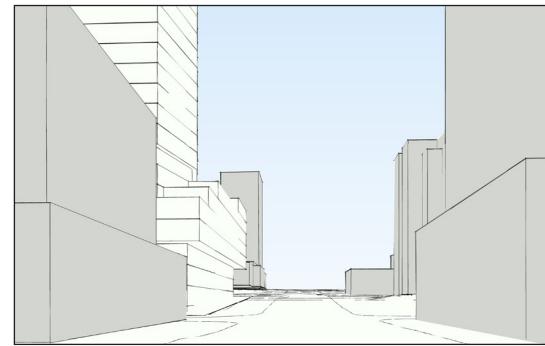
- Provide a minimum setback of 4m for the ground level façade comprised of either 1 level of commercial or 2 levels of residential.
- Provide an upper level façade along the majority of the building face located a minimum of 1.5m from the property line. This setback excludes balconies, cornices, guardrails and other architectural elements unless required to support the health of the established Horse Chestnut and Maple trees.
- The upper level façade shall transition from 5 storeys at the Yates Street corner to 3 storeys at the View Street corner.
- The street wall shall be no less than 4 storeys.
- Provide a minimum setback of approximately 6m for towers.
- Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 20m above grade from the property line parallel to the street up to 50m above grade. No further setbacks are required above 50m.



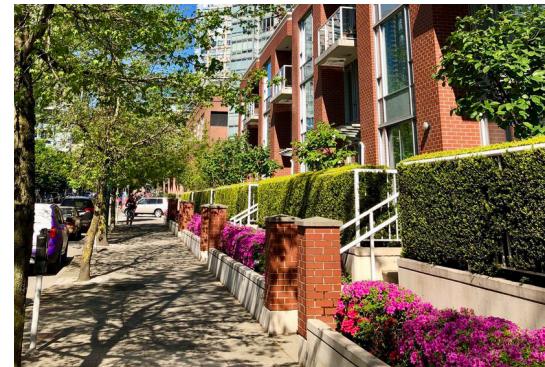
Key Plan



1. Bird's eye view looking north west



2. Looking north along Vancouver Street

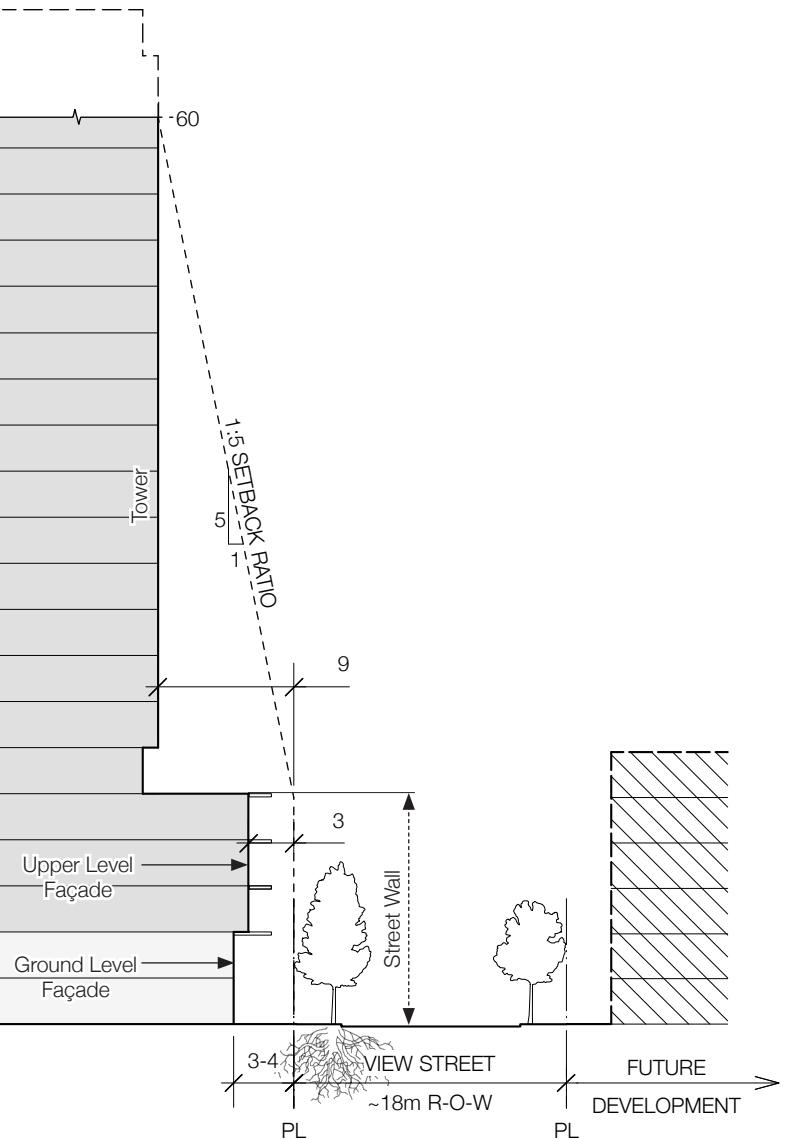


3.2.3 VIEW STREET

View Street is within a narrow right-of-way of approximately 18m in width. It is considered a 'local street' per the DCAP (2011). View runs east-west and allows two-way vehicle travel.

Guidelines:

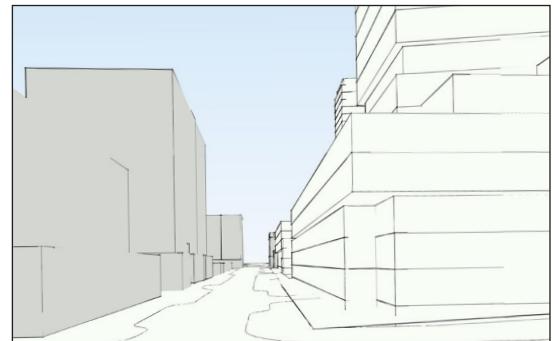
- a. Provide a minimum setback of 4m for a ground level façade comprised of 2 levels of residential; Provide a minimum setback of 3m for a ground level façade comprised of commercial uses.
- b. Provide an upper level façade along the majority of the building face located a minimum of 3.0m from the property line. This setback excludes balconies, cornices, guardrails and other architectural elements unless shown to be necessary to support the establishment of street trees.
- c. The upper level façade shall be 3 storeys along the length of View Street and is permitted to transition to 4 storeys at Cook Street.
- d. The street wall shall be no less than 3 storeys.
- e. Provide a minimum setback of approximately 9m for towers.
- f. Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 15m above grade from the property line parallel to the street up to 60m above grade. No further setbacks are required above 60m.



Key Plan



1. Bird's eye view looking east



2. Looking west along View Street

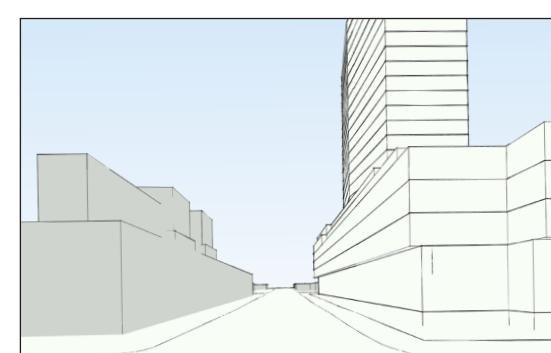
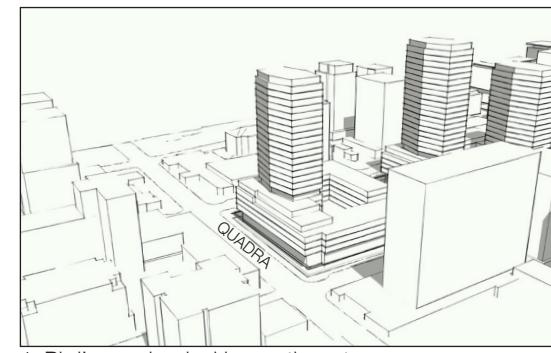
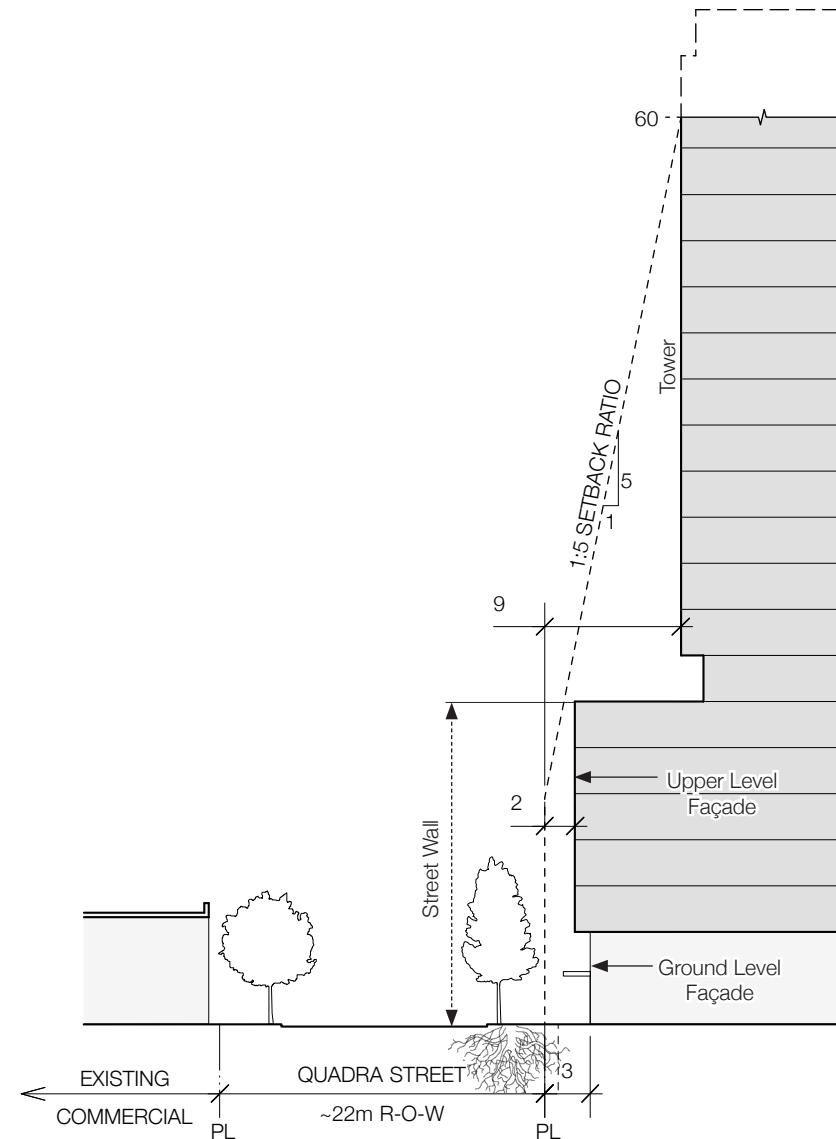


3.2.4 QUADRA STREET

Quadra Street is within a right-of-way of approximately 22m in width. It is considered a 'local street' per the DCAP (2011). Quadra runs north-south and allows two-way vehicle travel. It is also a public transit network route.

Guidelines:

- a. Provide a minimum setback of 3m for the ground level façade.
- b. Provide an upper level façade along the majority of the building face located a minimum of 2.0m from the property line. This setback excludes balconies, cornices, guardrails and other architectural elements unless shown to be necessary to support the establishment of street trees.
- c. The upper level façade shall transition from 5 storeys at the Yates Street corner to 3 storeys at the View Street corner.
- d. The street wall shall be no less than 3 storeys.
- e. Provide a minimum setback of approximately 9m for towers.
- f. Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 15m above grade from the property line parallel to the street up to 60m above grade. No further setbacks are required above 60m.

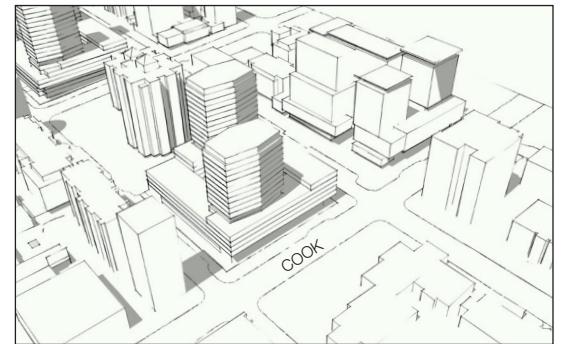
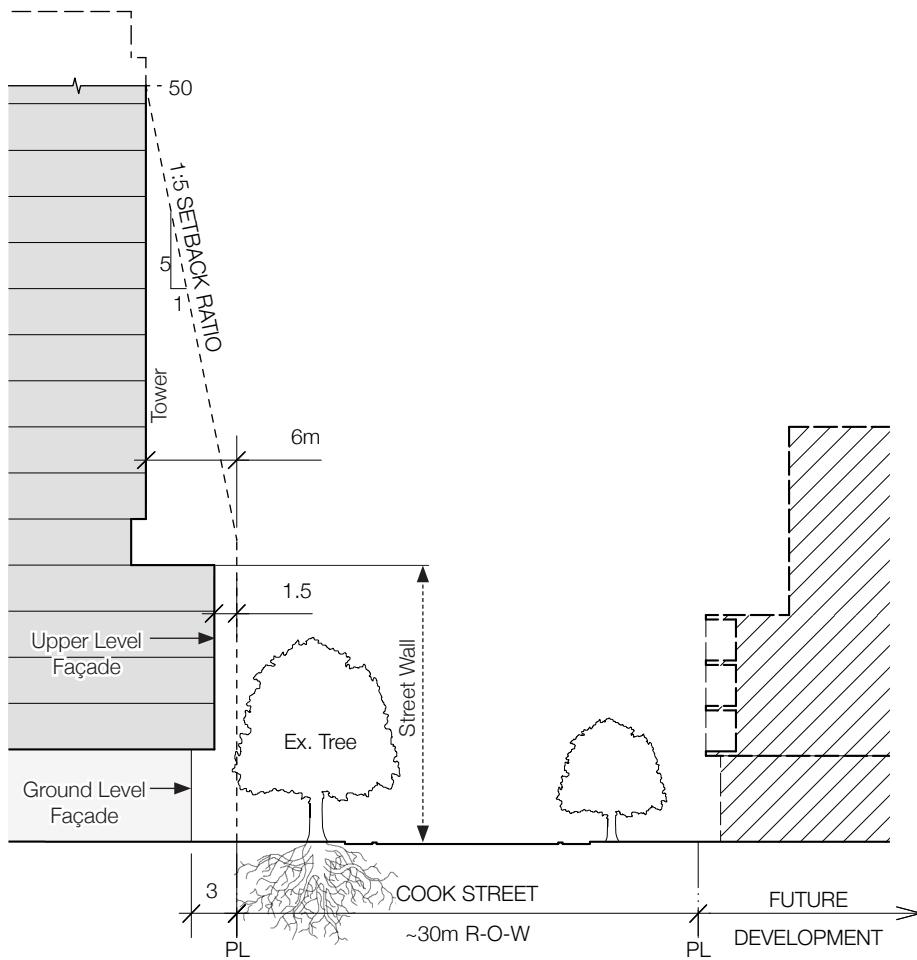


3.2.5 COOK STREET

Cook Street is within a right-of-way of approximately 30m in width. It is considered a 'Commercial street' per the DCAP (2011). Cook runs north-south and allows two-way vehicle travel. It is also a public transit route and is intended to be part of the AAA bicycle network in the long term. Four significant trees are located in linear boulevard in the right-of-way along the project frontage.

Guidelines:

- a. Provide a minimum setback of 3m for the ground level façade.
- b. Provide an upper level façade along the majority of the building face located a minimum of 1.5m from the property line. This setback excludes balconies, cornices, guardrails and other architectural elements unless required to support the health of the four established Horse Chestnut trees.
- c. The upper level façade shall transition from 5 storeys at the Yates Street corner to 4 storeys at the View Street corner.
- d. The street wall shall be no less than 4 storeys.
- e. Provide a minimum setback of approximately 6m for towers.
- f. Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 20m above grade from the property line parallel to the street up to 50m above grade. No further setbacks are required above 50m.



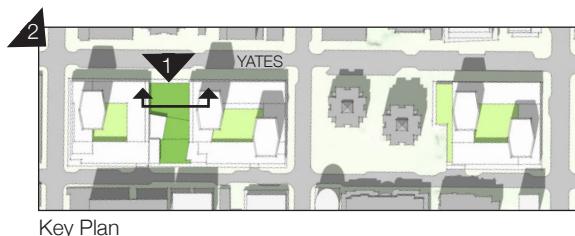
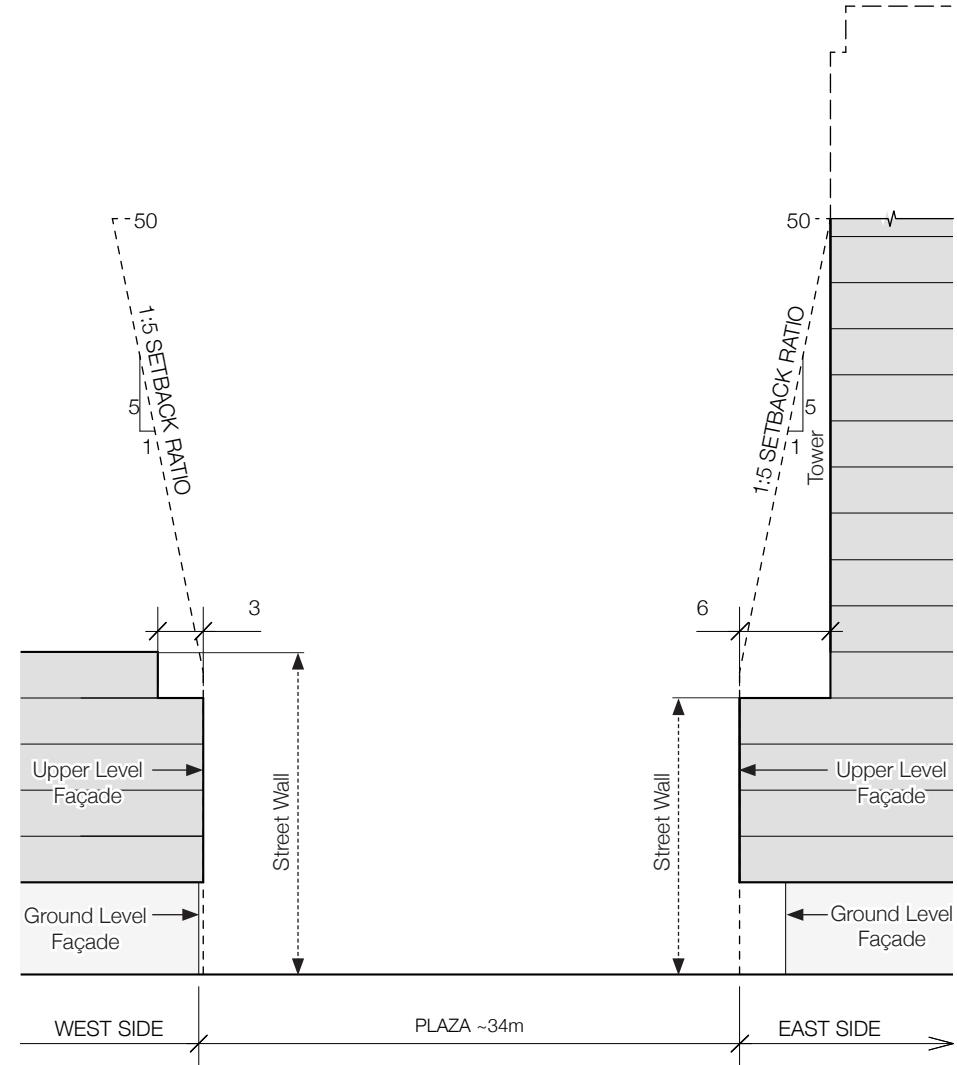
3.2.6 YATES STREET PLAZA

Intent: To create a plaza fronting Yates Street that is defined by building façades, animated by street level uses and, avoids as much as possible, overshadowing by adjacent buildings.

Guidelines:

(for buildings on the east and west sides of the plaza)

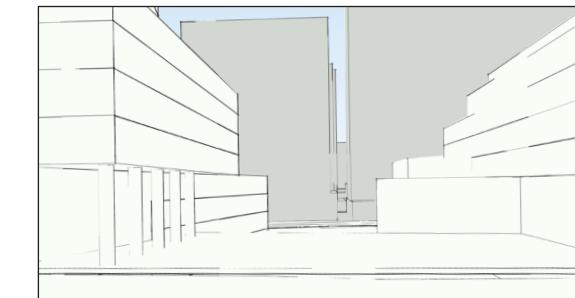
- a. The street wall shall be no greater than 6 storeys on the west side of the plaza.
- b. Provide a minimum setback of approximately 3m for the sixth storey on the west side of the plaza measured from the upper level façade.
- c. The street wall shall be no greater than 5 storeys on the east side of the plaza.
- d. Provide a minimum setback of approximately 6m for towers on the east side of the plaza measured from the upper level façade.
- e. Ensure that all portions of the building, excluding parapets, cornices, guardrails and other architectural elements are contained within a 1:5 building setback ratio established at 20m above grade up to 50m above grade. No further setbacks are required above 50m.



Key Plan



1. Bird's eye view looking south east



2. Looking south from the plaza to the green

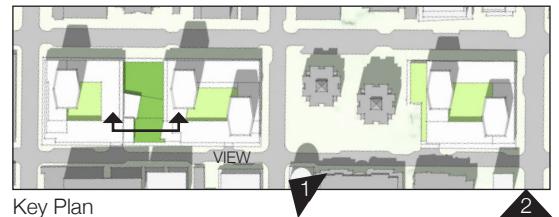
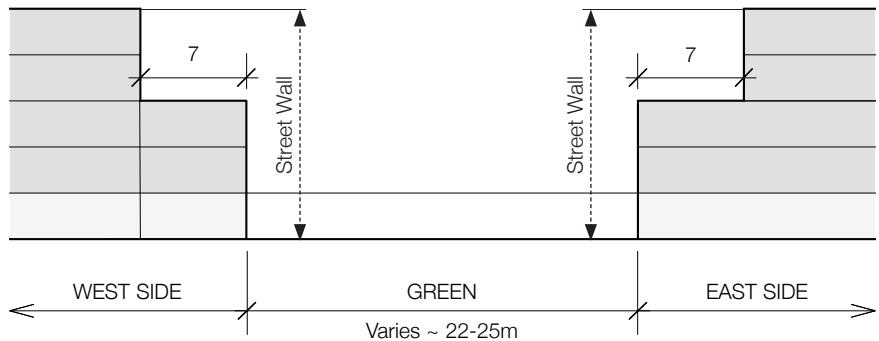


3.2.7 VIEW STREET GREEN

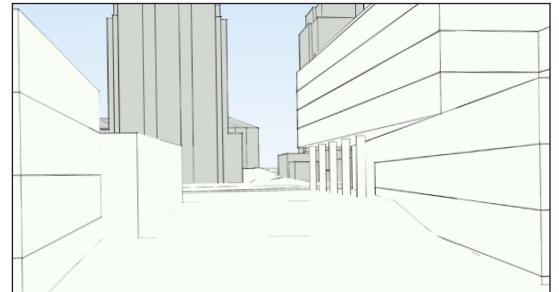
Design Intent: To provide a neighbourhood scale green space that is framed by residential townhouses and not overly shadowed by adjacent buildings.

Guidelines:
(for buildings on the east and west sides of the green)

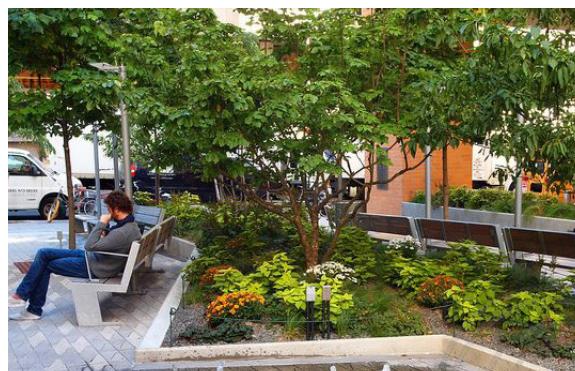
- The street wall shall be no taller than 5 storeys.
- A setback of approximately 7m or more from the street wall shall be provided above level 3.



1. Bird's eye view looking north west



2. Looking north from the green to the plaza



3.3 BUILDING + STREET INTERFACE

3.3.1 GROUND LEVEL USES

Design Intent: Ground-level uses contribute to a lively and interesting street network. Ground oriented residential uses are appropriate when commercial space is not in demand, ensuring a level of activity at the street level.

Design Strategies:

- a. Commercial and retail use;
- b. Residential lobbies;
- c. Ground oriented townhouses (Vancouver and View Streets).



Public space with commercial spaces at ground level, residential above

3.3.2 CHARACTER OF GROUND FLOOR (commercial/retail use)

Design Intent: Active, permeable, transparent, and welcoming. Animate the public street.

Design Strategies:

- a. Multiple entrances;
- b. Clear glazing;
- c. Sidewalk areas for retail display and casual moveable seating within the setback area.



Street furniture, lighting, clear glazing and trees contribute to a vibrant streetscape

3.3.3 CHARACTER OF GROUND FLOOR (residential use)

Design Intent: Active, pedestrian scaled, safe, and permeable.

Design Strategies:

- a. Provide primary street entrances for ground floor units;
- b. Opportunity for personal expression in some form (landscaping, furniture, etc);
- c. Clear glazing to allow observation of the street from the residence;
- d. Clear delineation of private space (grade change and landscaping, etc).



Individual entrances from the street, framed by landscaping

3.3.4 ENTRANCES/ EXITS

Design Intent: Welcoming, safe, and weather protected.

Design Strategies:

- a. Provide weather protection;
- b. Set residential lobbies back from the building face to allow sufficient space for arrival, egress and informal encounters and to help transition from the public street to the semi-private realm of the building;
- c. Differentiate residential lobby entrances from commercial entrances;
- d. Ensure entrances are clearly visible and accessible from the street or public space;
- e. Ground oriented residential units should have direct ground floor access and individual entry alcoves, patios or porches;
- f. Provide windows in exit stairs if adjacent to outside and permissible by the building code.



Commercial entrance framed by retail display on the street

3.3.5 SIDEWALKS (adjacent to commercial/retail use)

Design Intent: Generous sidewalk widths to accommodate various speeds of pedestrian movement and pedestrians with mobility devices.

Design Strategies:

- Extend the sidewalk area paving beyond the property line;
- Where sidewalks are adjacent to parallel street parking, additional sidewalk width is recommended;
- Provide a sidewalk area that is unencumbered by street furniture or objects, ie. sandwich boards, bicycle racks.



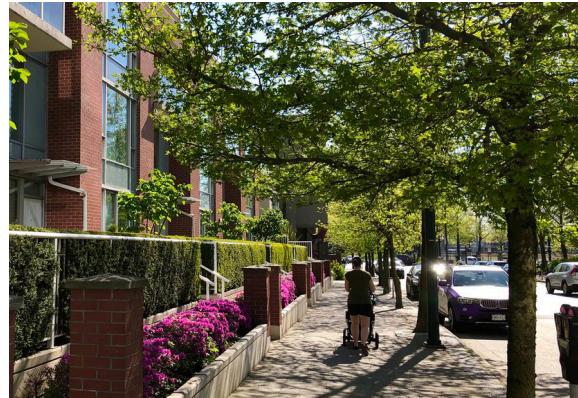
Clear sidewalk area adjacent to commercial uses

3.3.6 SIDEWALKS (adjacent to residential use)

Design Intent: Pleasant and human scaled sidewalks.

Design Strategies:

- Provide a sidewalk area that is unencumbered by street furniture or objects, ie. sandwich boards, bicycle racks.



Clear sidewalk area adjacent to residential uses

3.3.7 PARKING

Design Intent: To conveniently accommodate various modes of transportation.

Design Strategies:

- Short term street parking;
- Regularly spaced bicycle racks on every street, sized to accommodate standard bicycles and longer bicycles (cargo, tandem, longtail, bike + trailer, etc);
- Space for informal mobility device parking.



Bicycle parking area outside of clear sidewalk area; sidewalk paving extending beyond property line.

3.3.8 OPAQUE WALLS

Design Intent: Use unavoidable or intentionally windowless walls as an opportunity for creative expression.

Design Strategies:

- Minimize amount of blank walls
- Provide as a canvas for a public artwork;
- Create a vertical landscaped wall, subject to viability review by a landscape architect.



Public artwork enlivens the street

3.3.9 ROOFSCAPE

Design Intent: All remaining roof areas above the level two courtyards on the podium and towers are generally inaccessible for both the 900-block and 1045 Yates Street. However, patios and terraces opening onto the roof may be provided, and additional amenity uses may be provided at the option of the developer. Access to mechanical equipment and related building services will be maintained.

Design strategies:

- a. Light coloured and heat-reflecting ballast on all remaining roof areas to mitigate the urban heat-island effect.
- b. Screening of mechanical equipment to mitigate visual as well as fan noise.



Decorative, heat-reflecting ballast



Mechanical equipment screen



Roof treatment using light and reflective colours to minimize urban heat island effect



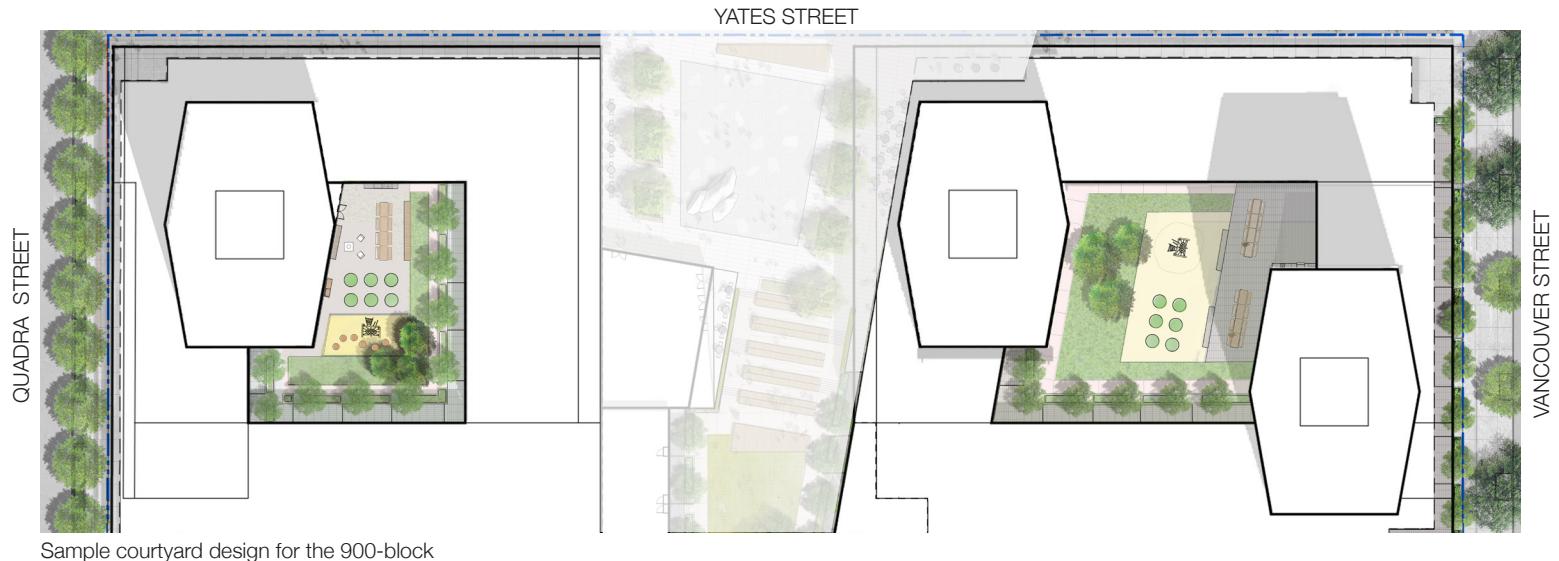
Screening of roof area to create privacy

3.3.10 COURTYARDS

Design Intent: To provide shared outdoor amenity space to residents and private patio space for units.

Design strategies:

- a. Screen private patios from the outdoor amenity areas, while connecting them with walkways to the amenity areas.
- b. Provide outdoor spill out areas for indoor amenity areas.
- c. Outdoor cooking and dining areas.
- d. Incorporate outdoor play areas with informal play elements for all ages. Clear site line will be provided for parents and caregivers from the amenity area.
- e. Urban agriculture, composting and potting area.
- f. A secured connection from the plaza to courtyard, if feasible.



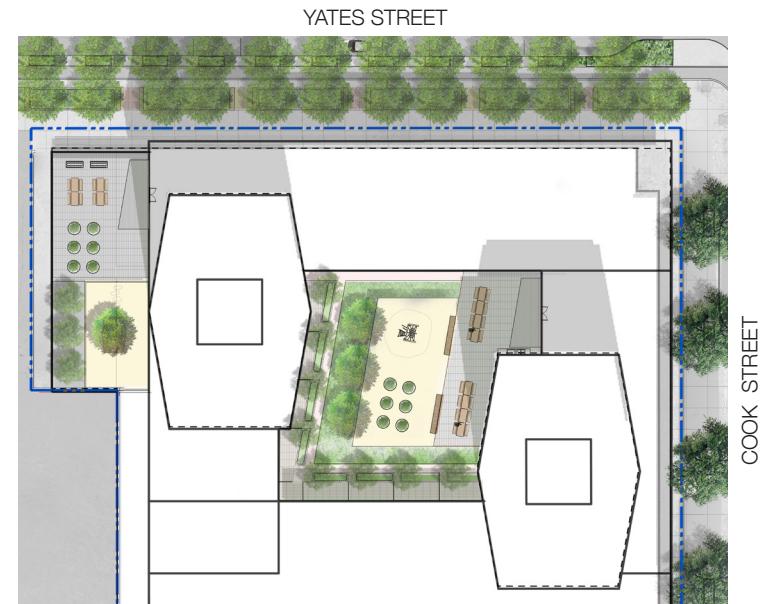
Planting in courtyard



Outdoor amenity area in courtyard



Informal play area in courtyard for all ages



Sample courtyard design for 1045 Yates

3.3.11 STREET FURNITURE

Design Intent: A suite of street furnishings may include custom and off the shelf products that complement the furnishings outlined in the Downtown Public Realm and Streetscape Plan.

Design Strategies (Seating):

- a. Fixed seating: benches in long and short formats, seating platform, and “Megabench”.
- b. Raised planters with bench seating.
- c. Seat steps at Harris Green Terrace.
- d. Moveable tables and chairs in Yates Street Plaza.



Megabench



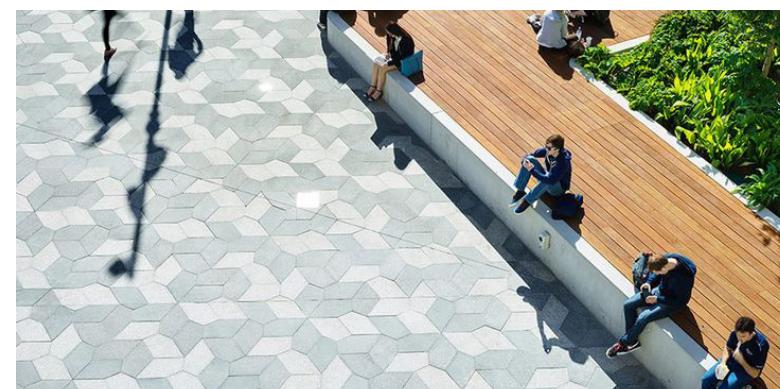
Seat steps



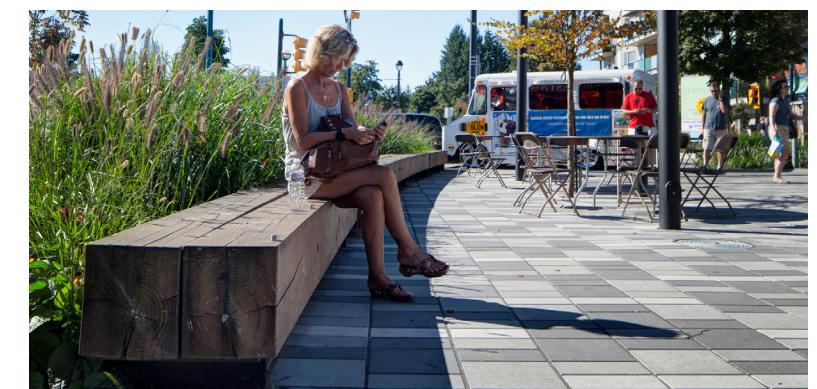
Benches and raised planters



Moveable tables and chairs



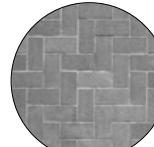
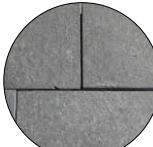
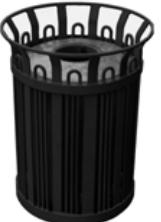
Platform seating



Long bench

Additional street furnishings:

- a. Pet comfort stations are located along Yates Street. The stations provide water fountains, waste bag dispensers and pets a place to go about their business without negatively impacting neighbourhood parks and green spaces. The pet comfort stations have soft porous surfaces (i.e. sand, cedar chips, rubber surfacing that encourage infiltration but can easily be maintained.
- b. Trash receptacles, bike racks, bollards, streetscape paving, tree grates etc. will be confirmed during design development, but will be based on the intent of the recommended furnishings in Downtown Public Realm and Streetscape Plan.

NEW TOWN PAVING MATERIALS			BOLLARDS	TRASH BINS
1 TROWEL JOINT CONCRETE	2 CONCRETE UNIT PAVERS	3 BASALT PAVERS	TYPE B	TYPE A
				
<ul style="list-style-type: none"> • Installation Method: Cast-in-place • Application: Sidewalk fill and frame • Colour: Natural • Finish: Fine broom finish 	<ul style="list-style-type: none"> • Dimensions: 225mm x 75mm x 60 • Installation Method: Mortar set • Application: Paving field • Colours: Natural grey • Finish: Unsealed 	<ul style="list-style-type: none"> • Dimensions: 300mm x 450mm x 60mm • Installation Method: Mortar set • Application: Entry banding • Colour: Charcoal grey • Finish: Flamed 	<p>Type B Bollard: Modern style</p> <ul style="list-style-type: none"> • Application: Corner bump outs and all other locations where vertical separation is needed 	<p>Type A: Modern Metal Bin</p> <ul style="list-style-type: none"> • Application: All locations • Multi-stream recycling receptacles are to be used when appropriate on a case by case basis

Downtown Public Realm Plan & Streetscape Standards



Urban dog run



Pet comfort station amenities



Pet comfort station material inspiration

3.3.12 LIGHTING

Design Intent: Exterior lighting will be an integral feature of the architecture, landscape design and streetscape and can be an attraction in itself. A combination of the following lighting strategies will provide nighttime, event and seasonal lighting. Lighting not only improves safety for pedestrians and vehicular traffic, but can delineate the plaza and surrounding streetscape as a special place and a gateway into downtown. Lighting can extend the use of the streets and plaza into the evenings and the darker winter season.

Design Strategies:

- Specialty plaza lighting will act as a focal point for the plaza. This type of lighting can be suspended over the plaza or featured in the ground plane.
- Catenary lighting can act as an inviting feature in the plaza or streetscape.
- Pedestrian scale lighting provides safety and security while guiding pedestrians through the site.
- Soffit lighting can be integrated into recesses in building overhangs to provide lighting to the building frontage and the streetscape and plaza areas surrounding the building.
- Vehicular lighting improves safety for vehicular traffic and pedestrians alike.



Specialty lighting



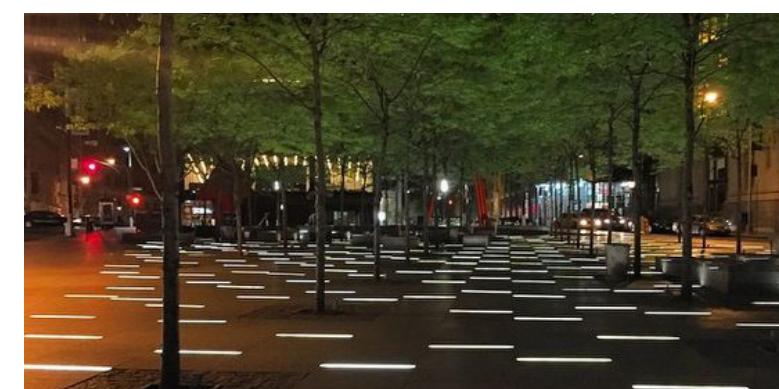
Combination of catenary, pedestrian scale, soffit and plaza lighting



Under bench lighting



Catenary lights



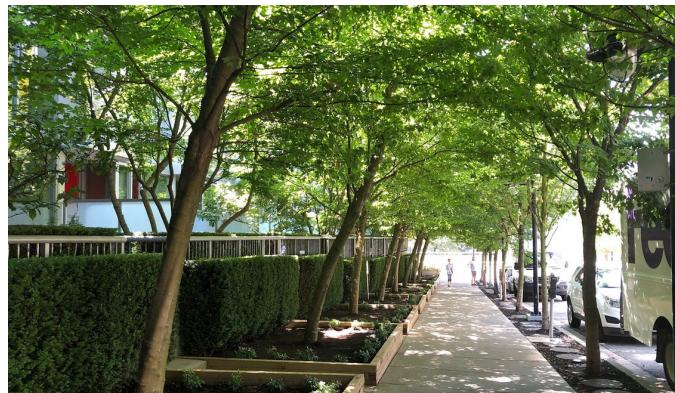
Plaza lighting



Plaza lighting

3.4 URBAN ECOLOGY

Design Intent: We define urban ecology as a systems approach to harnessing ecosystem services in the urban context. For Harris Green Village urban ecology consists of a three pronged approach. The design intends to use trees and tree canopy, planting in the form of rain gardens and stormwater management to minimize the development's reliance on existing stormwater infrastructure and offer weather protection and favourable microclimates throughout the seasons to end users. The tree, planting and stormwater strategies are described below.



Double row of trees in Downtown Vancouver



Double row of trees on Hornby Street, Downtown Vancouver

3.4.1 TREES

Design Intent: Provide appropriate tree species for a full street tree canopy and key specimen trees in the Yates Street plaza, View Street Green and courtyards.

Design Strategies:

- a. Appropriate tree species will be selected to best match the hydrological conditions on Yates, Quadra and View Streets. Rain gardens will be located along these streets where the conditions and sidewalk and street width allow.
- b. Tree species and cultivars will be selected as recommended in the Downtown Public Realm Plan & Streetscape Standards for the New Town District whenever possible (i.e. medium to large size, round to broad form, transparent canopy).
- c. The existing healthy horse chestnut trees on Vancouver and Cook streets will be maintained and supplemented with a matching or complementary tree species.
- d. Double row of trees on Yates Street, one row in standard street tree grates and a second row planted in rain gardens. To provide a buffer and separate pedestrians from vehicle and bike traffic.



Styrax japonicus
Japanese snowbell



Cercidiphyllum japonicum
Katsura



Zelkova serrata
Japanese zelkova



Aesculus x carnea
Red flowering horse chestnut

SIZE	FORM	CANOPY
LARGE	COLUMNAR	DENSE
MEDIUM	ROUNDED	BROAD
SMALL		

Downtown Public Realm Plan & Streetscape Standards

3.4.2 PLANTING

Design Intent: Robust planting areas that are not only attractive, but absorb stormwater infrastructure, provide comfort for pets, soften and buffer traffic and hardscape elements. The plant palette is comprised of west coast native and adaptive species featuring rain garden planting

Design Strategies:

- Rain garden planting to capture, slow, infiltrate and convey stormwater primarily on Yates, Quadra and View Streets where conditions and sidewalk and street widths allow.
- Lush, robust planting with seasonal interest in the Yates Street Plaza and View Street Green in raised planters. Plant species to include native, adaptive, and rain garden plant species.
- Vertical planting on walls and/or trellises to screen and soften blank walls.
- Pet comfort areas bordered by pet friendly robust planting.



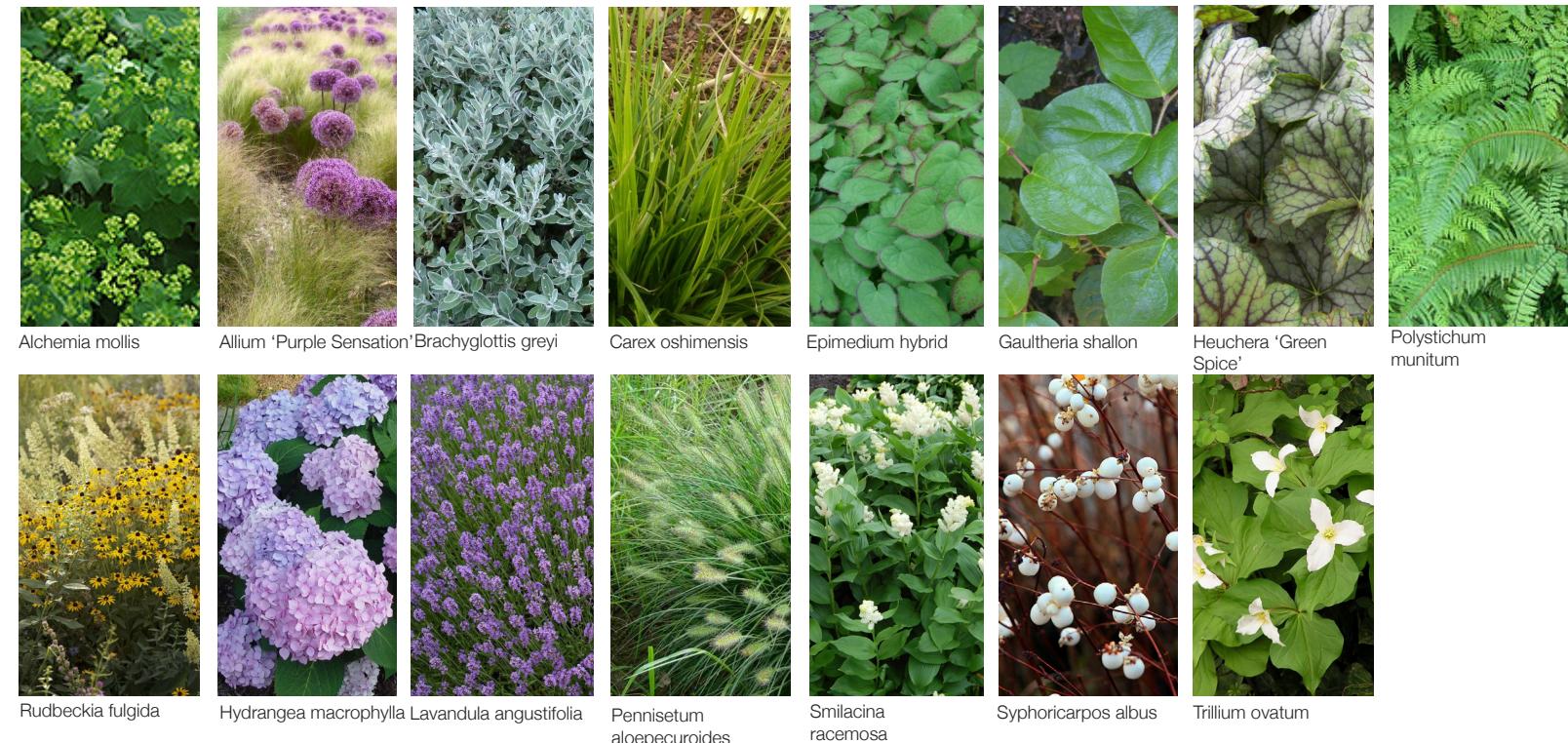
Rain garden planting



Green wall/vertical planting

Sample plant palette for shrubs, groundcovers & grasses:

<i>Alchemia mollis</i>	<i>Lady's Mantle</i>	<i>Lavandula angustifolia</i>	<i>Lavender</i>
<i>Allium hollandicum</i>	<i>'Purple Sensation' Flowering Onion</i>	<i>Liriope muscari</i>	<i>Lily Turf</i>
<i>Arctostaphylos uva ursi</i>	<i>Kinnickinnick</i>	<i>Lonicera pileata</i>	<i>Box-leaf Honeysuckle</i>
<i>Brachyglottis greyi</i>	<i>Daisy Bush</i>	<i>Pachysandra terminalis</i>	<i>Japanese Spurge</i>
<i>Campanula portenschlagiana</i>	<i>Bellflower</i>	<i>Pennisetum alopecuroides 'Hameln'</i>	<i>Dwarf Fountain Grass</i>
<i>Carex caryophyllea 'The Beatles'</i>	<i>The Beatles Spring Sedge</i>	<i>Polystichum munitum</i>	<i>Sword Fern</i>
<i>Carex oshimensis 'Everlime'</i>	<i>Everlime Sedge</i>	<i>Polystichum setiferum</i>	<i>Shield Fern</i>
<i>Escallonia 'Newport Dwarf'</i>	<i>Escallonia</i>	<i>Prunus laurocerasus 'Mount Vernon'</i>	<i>Dward English Laurel</i>
<i>Epimedium</i>	<i>Barrenwort</i>	<i>Rosa rugosa 'Fru Dagmar Hastrup'</i>	<i>Single Pink Old Fashioned Rose</i>
<i>Gaultheria shallon</i>	<i>Salal</i>	<i>Rudbeckia fulgida</i>	<i>Rudbeckia</i>
<i>Hakonechloa macra</i>	<i>Japanese Forest Grass</i>	<i>Saxifraga 'Primuloides'</i>	<i>Miniature London Pride</i>
<i>Helleborus orientalis 'Royal Heritage'</i>	<i>Lenten Rose</i>	<i>Smilacina racemosa</i>	<i>False Solomon's Seal</i>
<i>Heuchera 'Green Spice'</i>	<i>Coral Bells</i>	<i>Trillium ovatum</i>	<i>Coast Trillium</i>
<i>Hydrangea macrophylla</i>	<i>Big leaf hydrangea</i>	<i>Vaccinium ovatum 'Thunderbird'</i>	<i>Evergreen Huckleberry</i>



3.4.3 STORMWATER MANAGEMENT

Design Intent: Maximize stormwater strategies that capture, convey, intercept, infiltrate and transpire stormwater on site are a key feature of this development.

Design Strategies:

- a. The removal of the existing trees will allow for the introduction of a double row of street trees set in rain gardens and tree pits. The removal of the existing trees will be required to allow for adequate space for rain gardens along Yates, View and Quadra Streets.
- b. Rain gardens planted with native and adapted species can assist in the capture and infiltration of rain water.
- c. The double row of trees on Yates and a dense tree canopy along the surrounding streets can exponentially increase rainfall interception as well as infiltration and evapotranspiration rates.
- d. Soil cells may be employed to provide tree roots with sufficient access to soil to ensure that trees are given the best possible chance at survival, improve growth outcomes and overall size. The soil cells also promote rain water collection, infiltration and root uptake.
- e. Rain leaders can collect and convey stormwater from the building to infiltration galleries below the rain gardens and street tree planting.



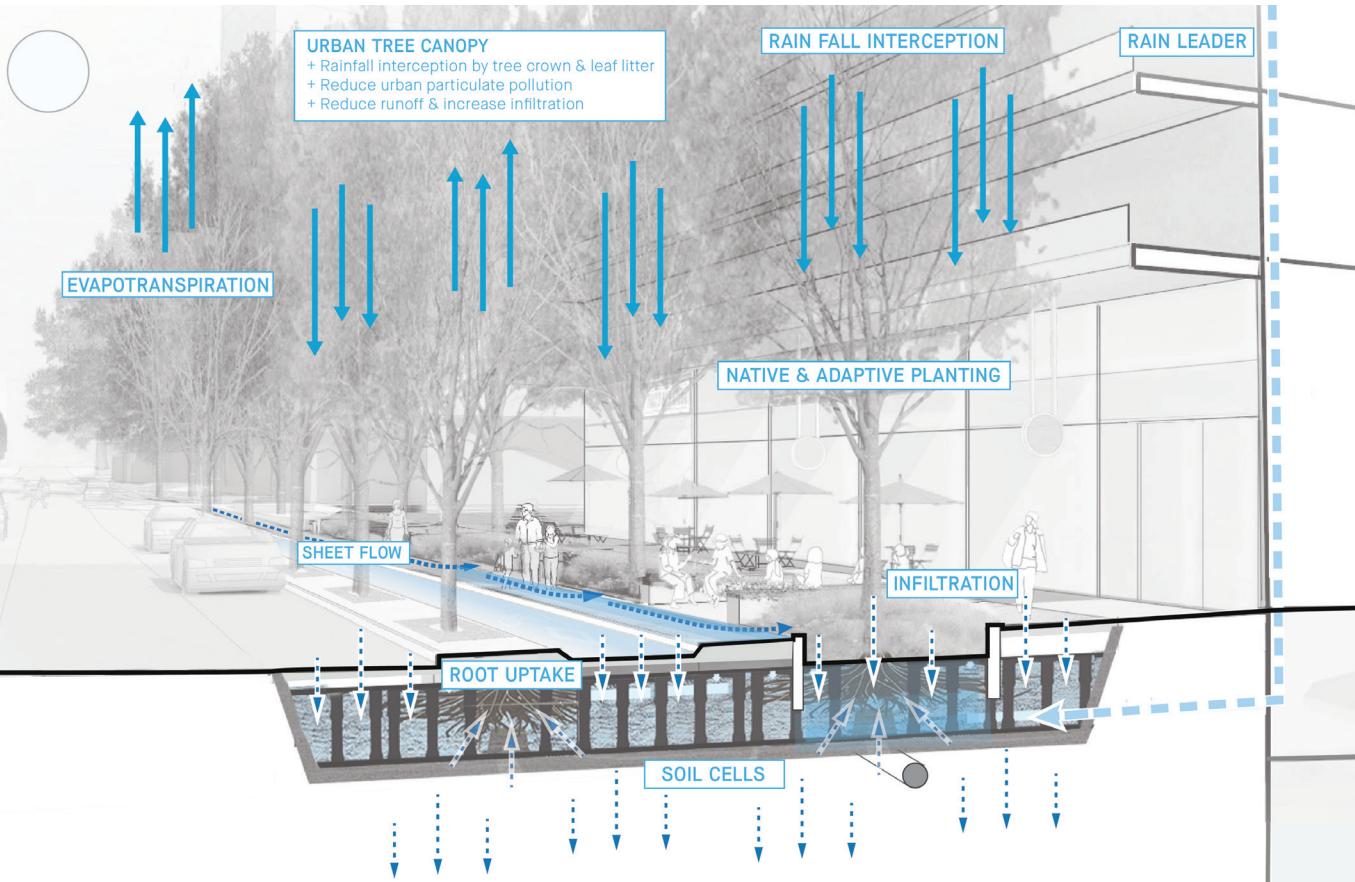
Rain garden



Rain garden



Soil cell installation



3.5 STREET and OPEN SPACE

The following images show examples of how ideas embodied in the preceding guidelines could be applied. They are not intended to be prescriptive nor definitive. Detailed design for each street will be developed by design professionals, in consultation with City of Victoria staff and in conformance with pertinent policies and statutes, as part of the Development Permit and Building Permit procedures for each phase.

3.5.1 YATES STREET

Yates Street is envisioned as a vibrant shopping street, with a double row of trees creating a continuous tree canopy, rain gardens and wide sidewalks. Street level activity includes restaurants, cafes, shops, residential and commercial lobbies. The future separated bicycle lane along Yates contributes to the active transportation options for the residents and users of the Harris Green Village.

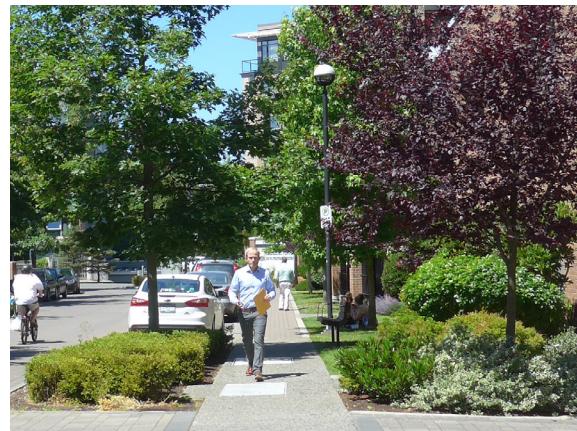
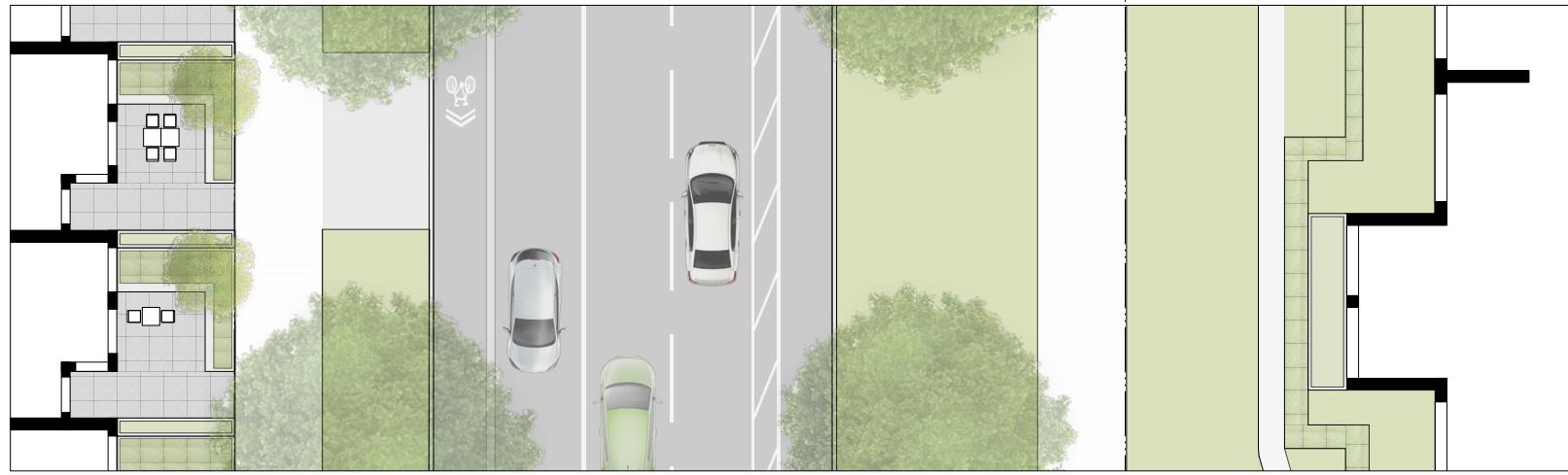
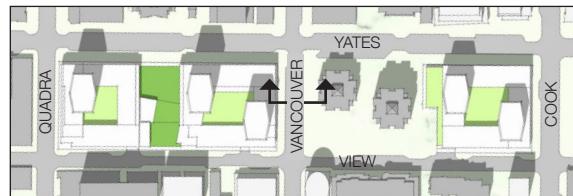


Lively street frontage

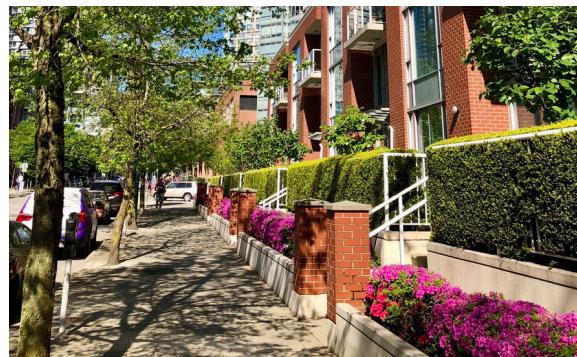


3.5.2 VANCOUVER STREET

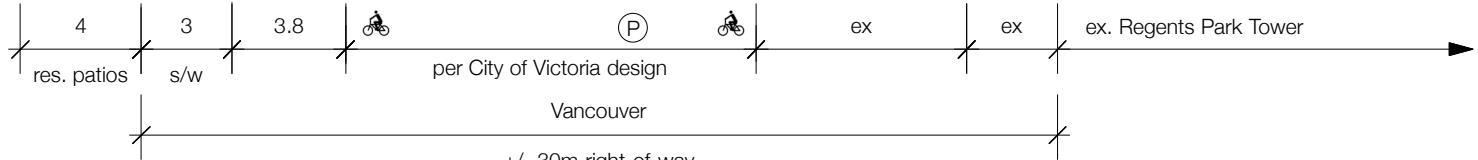
The proposed townhouses along Vancouver Street are intended to become part of an emerging, mixed-residential streetscape. There will be some commercial shops that will turn the Yates Street corner. The wide sidewalks, healthy Horse Chestnut and Maple trees and plants along Vancouver Street, are intended to be preserved. The townhouse entrance porches will be elevated from the sidewalk and large enough for personalized use. These private outside spaces will be separated from moving traffic by the wide sidewalk and future bicycle lane.



Wide, tree-lined sidewalk



Ground oriented residential

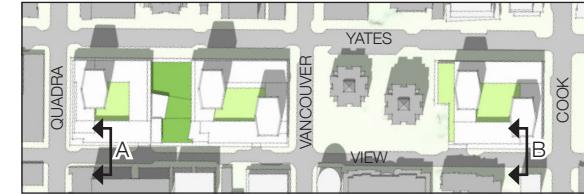


3.5.3 VIEW STREET

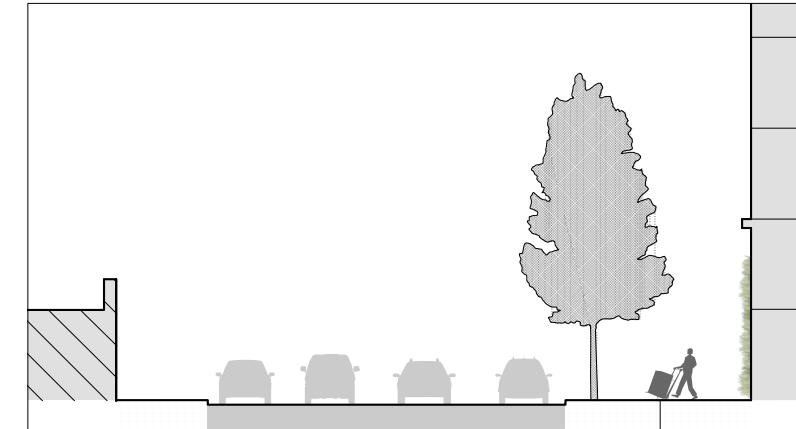
View Street will also have townhouses with entrances along the sidewalk. Sharing this long block will be entrance lobbies for the apartment podium and towers above, and portals to underground parking. Significantly, the mid-point of this long, south-facing frontage will be divided by the View Street Green with its flanking, east and west-facing townhouses it will add a new green space and public pedestrian route to the new Plaza and Yates Street to the north. Similar to Vancouver Street, the townhouse entrance porches will be elevated from the sidewalk and large enough for personalized use. A continuous row of trees, a clear sidewalk zone and planted areas create a pleasant and calm pedestrian environment. Any windowless expanses of wall that result from grade differences are seen as an opportunity and must be thoughtfully designed and considered part of the architectural and landscape composition.



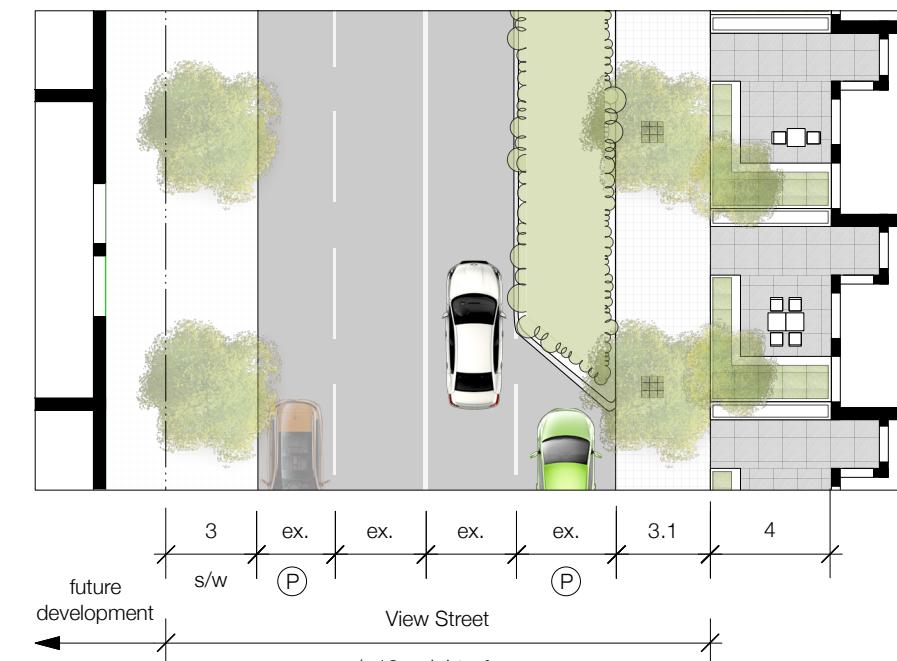
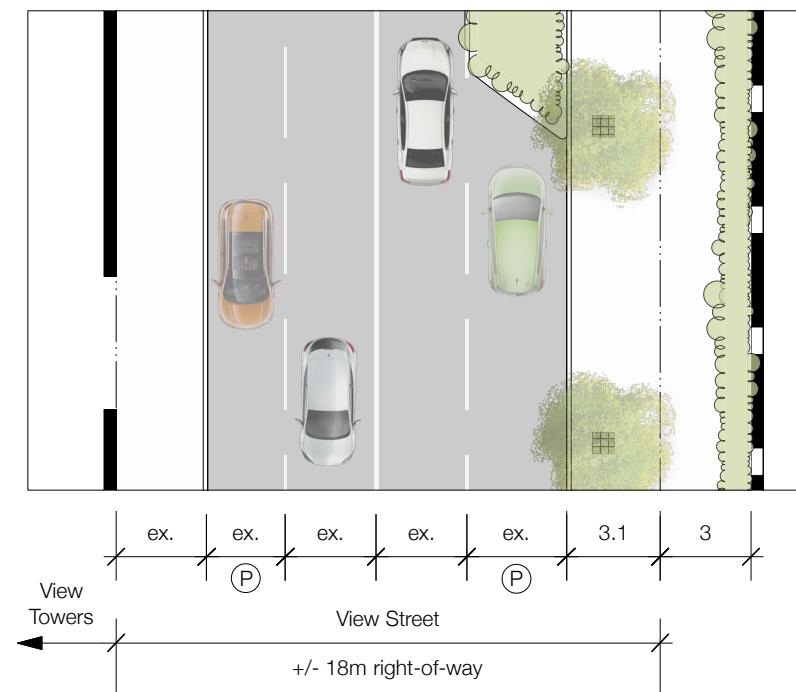
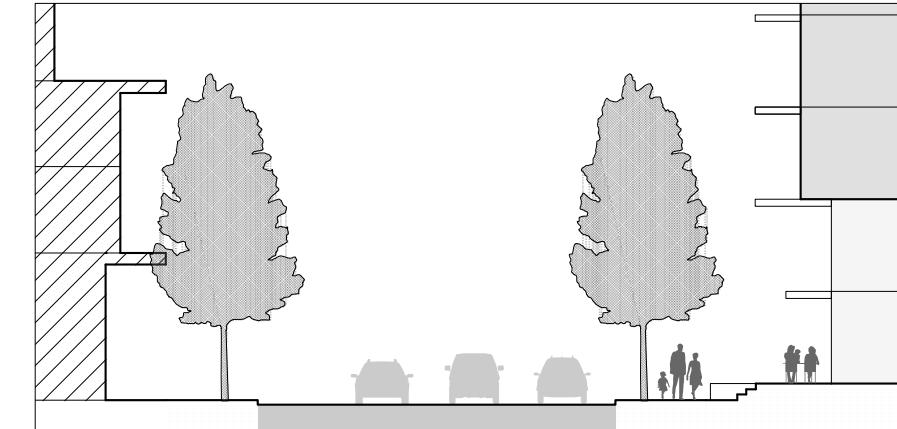
Attractive pedestrian environment



View Street (commercial/ service frontage) - A



View Street (residential frontage) - B



3.5.4 QUADRA STREET

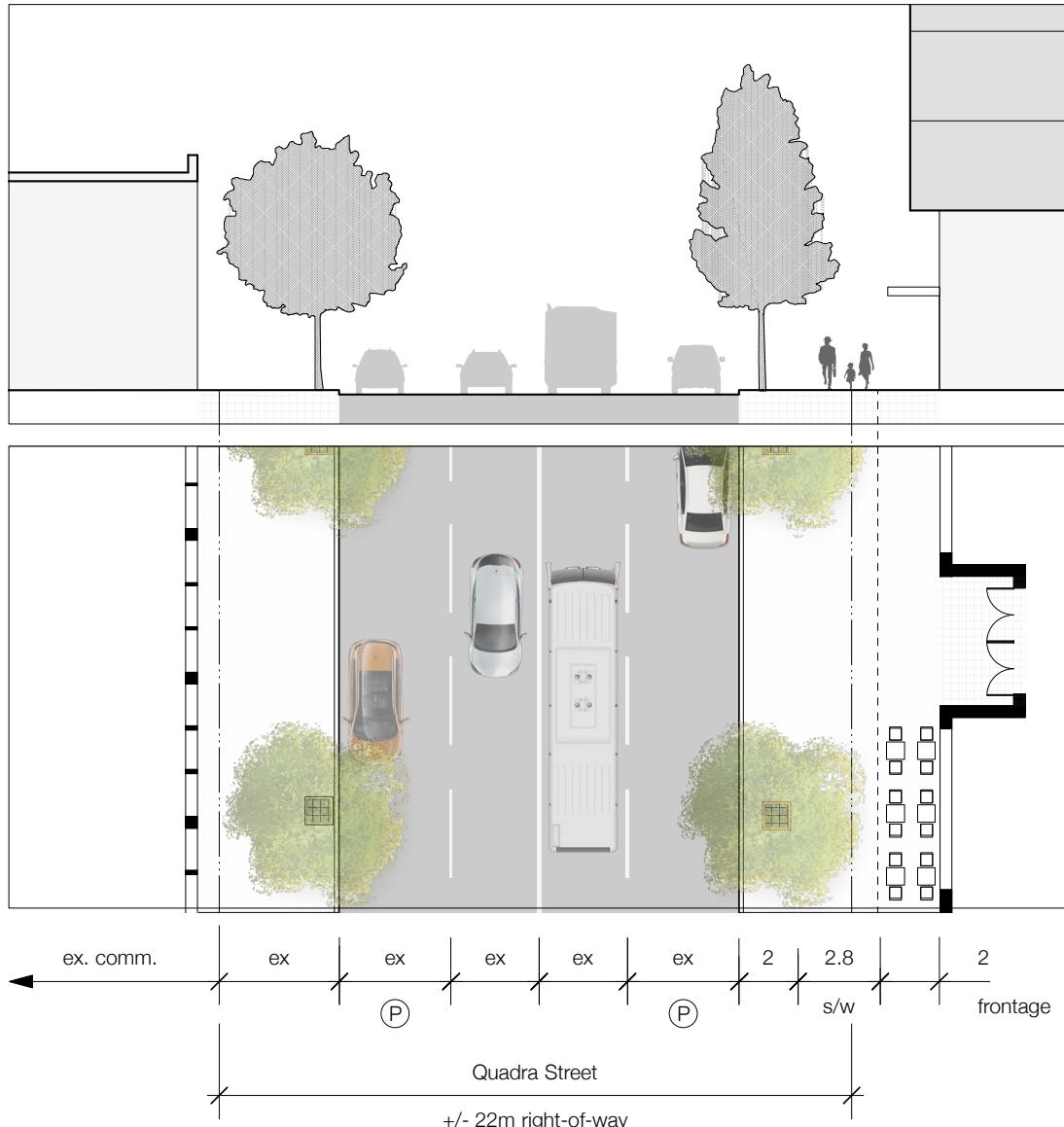
Quadra Street is envisioned to be a minor commercial street lined with shops sharing sidewalk-level access with entrance lobbies of residential apartments above. Wide sidewalks, space for commercial activity to spill onto the street, a continuous row of trees and wide sidewalks will create an interesting vitalizing pedestrian environment.



Transparent glazing provides glimpses to the interior activity



Seating, signage and plantings animate the sidewalk



3.5.5 COOK STREET

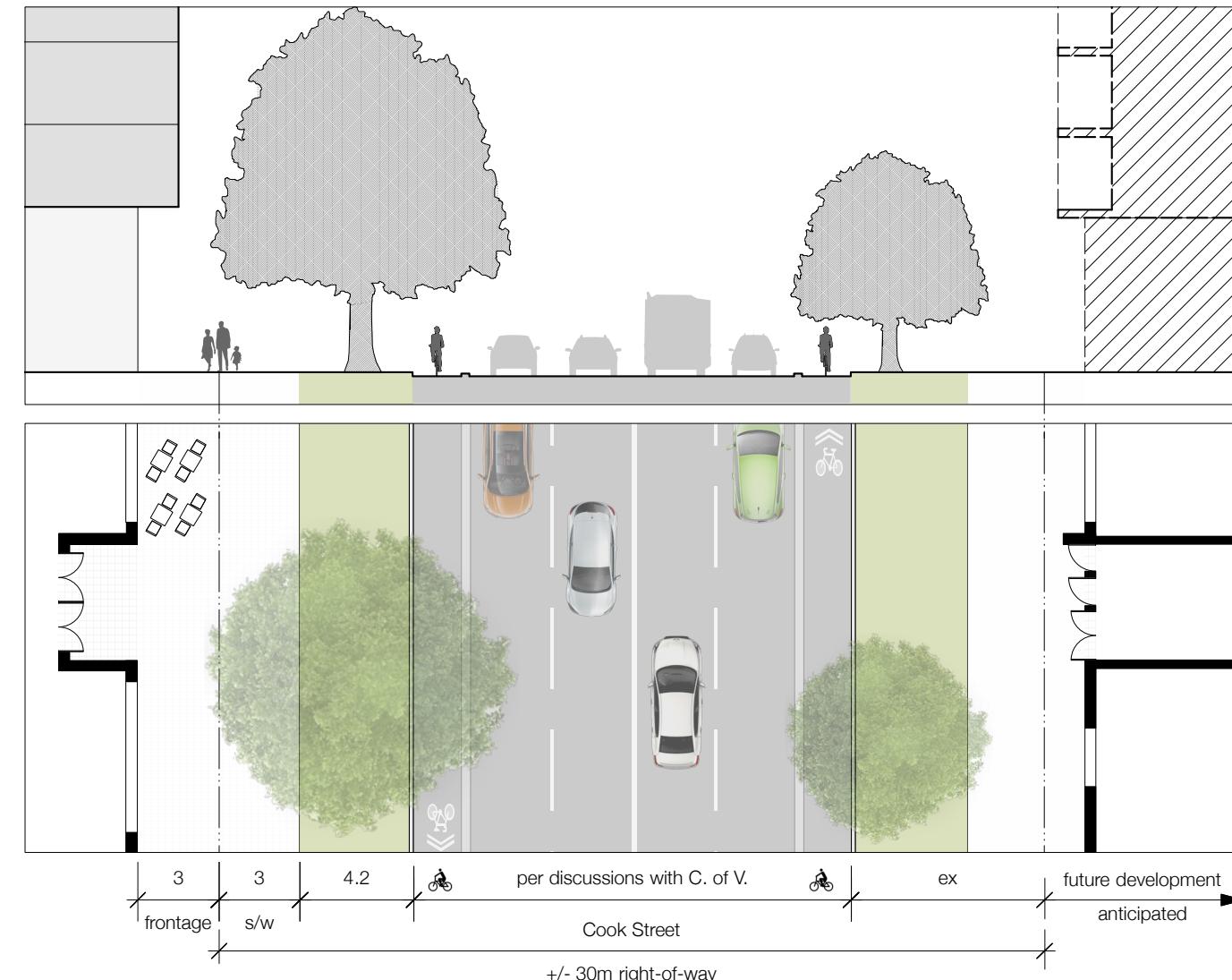
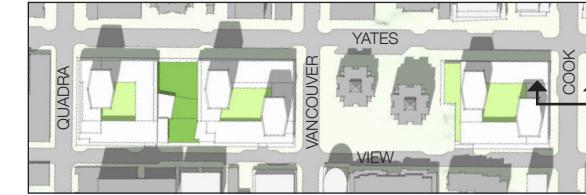
Cook Street is envisioned as a vibrant shopping street, with an established tree canopy, new rain gardens and wide sidewalks. Restaurants and café patios, shops and residential entrance lobbies will contribute to vitality. The future separated bicycle lane along Cook will extend the active transportation network for the residents and users of the Harris Green Village.



Cyclists, pedestrians, dogs, and rain gardens animate the street



Rain gardens manage and clean rainwater while creating buffers between pedestrians and moving vehicles



3.6 ARCHITECTURE

The DCAP's urban design guidelines embody ideas that express certain values and preferences that have become priorities for the community. These include:

- Respect for and adherence to the human-scaled, orthogonal grid of streets, originally layed out in the 19th century;
- Street rights-of-way defined by aligned building façades;
- Building height-to-street width proportions of the City's rights-of-way; and
- Building façades positioned along the streets so as not to loom over the street nor reduce visibility of the sky to a narrow slot.

To achieve these priorities and respond to the need and desire to accommodate increasing population in an environmentally and economically responsible and socially positive form, the typology selected for the architectural massing of the Harris Green Village project is the so-called podium-and-tower form. Known also as platform or pedestal type structure, podium-tower buildings consist of a relatively low-rise structure usually aligned around the perimeter of a city block, with a tall tower of stacked, relatively smaller floor-plates on top.

This form allows the streets to be defined by buildings of an appropriate height relative to the street width, and for a building containing larger amounts of floor space to be positioned set back and away from the street right-of-way, thereby achieving street definition without overly imposing constriction of the public realm.

The guidelines in this manual are predicated on employing the podium-tower architectural concept.

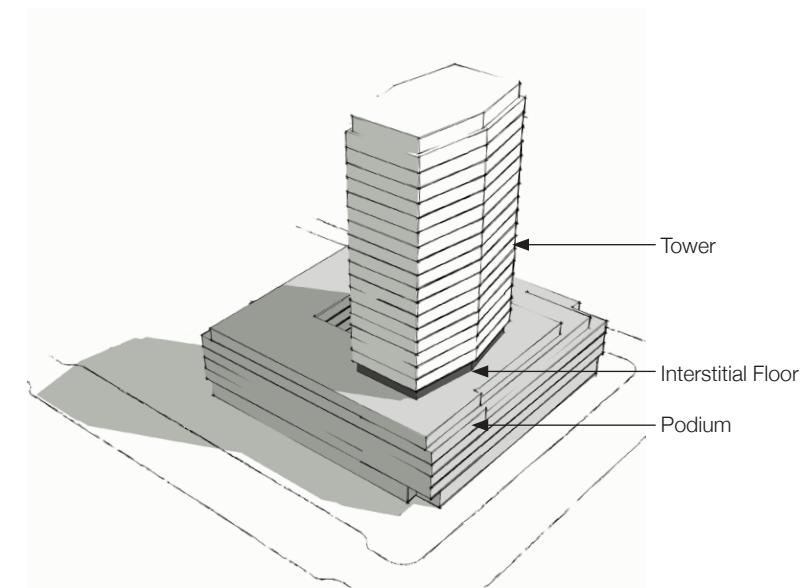
3.6.1 BUILDING TYPOLOGY

Intent:

To establish a tower on podium typology that distinguishes the project, enhances the skyline and contributes positively to the public realm.

Guidelines:

- a. To avoid the stepped massing (tiered wedding cake form), an interstitial floor or other architectural device, should be designed to articulate the connection and formally distinguish between podium and tower.
- b. Single floor plate area for tall buildings should not exceed 650 m²
- c. The tallest building in the project should be located in the centre of the 900-block and positioned to front Yates Street. Remaining towers should decline in height from Quadra Street down to Cook Street
- d. The street front façades of buildings should:
 - Provide a first floor height to accommodate commercial uses and residential lobbies.
 - Consist of well-defined entrances and active uses, wherever possible.
 - Use non-reflective, transparent glass windows along the street interface to allow for unobstructed views in and out of commercial spaces.
 - Include building overhangs and or canopies to provide weather protection along commercial streets and plazas. Canopies should be incorporated into the architecture of the building.
- e. The tower portion of a building should:
 - Be shaped and articulated to achieve a visually slender profile and height-to-width proportion.
- f. The design of tall buildings should:
 - Contribute to the quality and character of the skyline.
 - Integrate roof-top mechanical, telecommunications, sustainability features etc. into the design and massing of the upper floors.

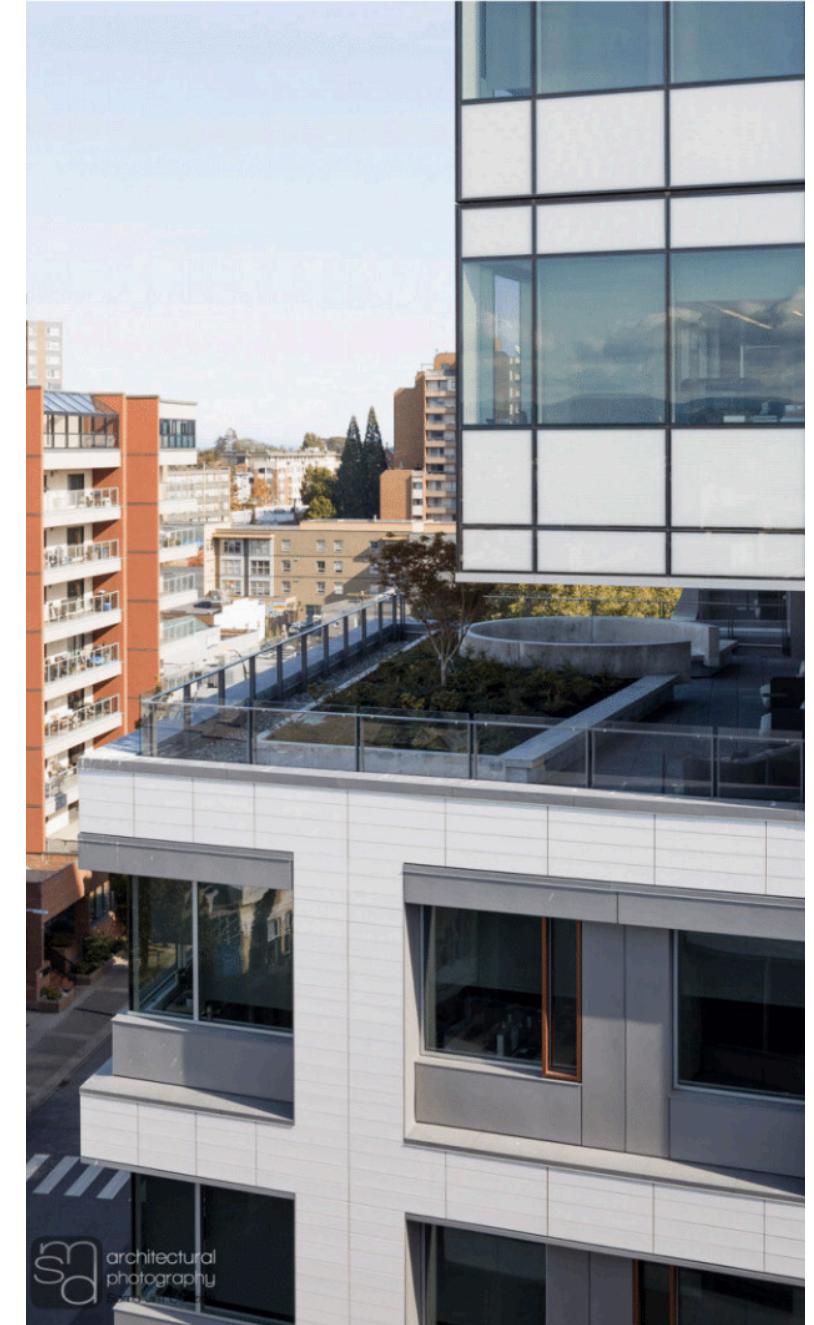




Podium-tower typology



Slender tower massing



Tower set back from podium



Transparency and integrated canopies



Active podium base



3.6.2 GROUND FLOOR TOWNHOUSES

Intent: Invigorate and enhance security of public streets with adjacent residential activities.

Guidelines:

Ground floor residential uses should:

- a. Provide individual entrances with connections to the public realm.
- b. Be set back from the property line to allow for a private patio or porch space.
- c. Be defined by a private patio space screened with low fencing and or landscape elements.
- d. Be raised up from ground level to enhance privacy.
- e. Provide transparency with windows to permit views between the public and private realm.
- f. Be designed with clear definition between private and public space.
- g. Be separated from adjacent parking or commercial entrances visually and with physical barriers or other devices to ensure pedestrian safety.



Ground Floor Residential

3.6.3 SHARED RESIDENTIAL APARTMENT ENTRANCES

Intent: Clearly identify and differentiate the entrances for multi-unit residential buildings.

Guidelines:

- a. Entrances should be clearly identifiable from the street or plaza they face.
- b. Residential lobbies should be differentiated from commercial entrances with architectural elements, lighting, signage, artwork or landscape features.
- c. Incorporate a high degree of transparency to permit visual connection between the public and private realm.
- d. Be inset from the street and incorporate weather protection to provide an area of refuge.
- e. Be universally accessible, providing automatic openers and adequate widths to accommodate mobility devices (wheelchairs, scooters, etc.).
- f. Canopies to be design to be easily maintained.



Common Residential Entrance Components

3.6.4 GROUND FLOOR COMMERCIAL SHOPS

Intent: Incorporate architectural elements that help animate and give visual interest to the public realm.

Guidelines:

Commercial spaces at the ground level should:

- a. Provide each commercial unit with an entrance directly from the public realm.
- b. Incorporate modularity to allow for multiple commercial units or a larger commercial user to consolidate units while maintaining the rhythm of multiple storefronts.
- c. Provide ample transparent glazing to permit views between the public and private realm.
- d. Incorporate weather protection through building overhang or integrated canopies.
- e. Be level with the sidewalk to allow barrier-free access to the public realm.
- f. Be universally accessible, providing automatic openers and adequate widths to accommodate mobility devices (wheelchairs, scooters, etc.).
- g. Encourage commercial-retail tenants to maintain ample views into retail spaces and avoid opaque graphics and other obscuring of windows to the street.



Ground Floor Commercial Components

3.6.5 VEHICLE ACCESS, PARKING & LOADING

Intent: Manage resident and service vehicle traffic in and around the site to prioritize pedestrian safety, reduce emissions and minimize impact on the public realm.

Guidelines:

- a. All vehicular parking and services should be underground with access via three entries, two from View Street for the 900-block and one from View Street for the 1045 Yates.
- b. Parking portals should be visually diminished through the use of recesses, trellis, screens, walls and landscape, while maintaining adequate egress for service vehicles and sight line safety for pedestrians.
- c. Emergency vehicle access must be provided to the satisfaction of the City and Fire Department. If necessary, emergency access routes should be integrated into the design of the plaza with paving, bollards and other features consistent with the palette of street furnishings.



Parkade entry screening

3.6.6 OPAQUE WALLS

Intent:

Minimize amount of unavoidable or intentional opaque walls at street-level.

Guidelines:

- Wherever possible, provide uses on the ground floor that are compatible with the provision of windows and doors that connect interior activities with the street level.



Generous glazing at the street level

3.6.7 MECHANICAL EQUIPMENT

Intent:

Minimize the visual and noise impact of mechanical equipment.

Guidelines:

- Screen rooftop mechanical equipment for acoustic mitigation and appearance with materials that are integrated with the design of the building
- Locate mechanical equipment, service areas and vents away from windows and people wherever possible and design to mitigate visual obtrusiveness and excessive noise or air flows.

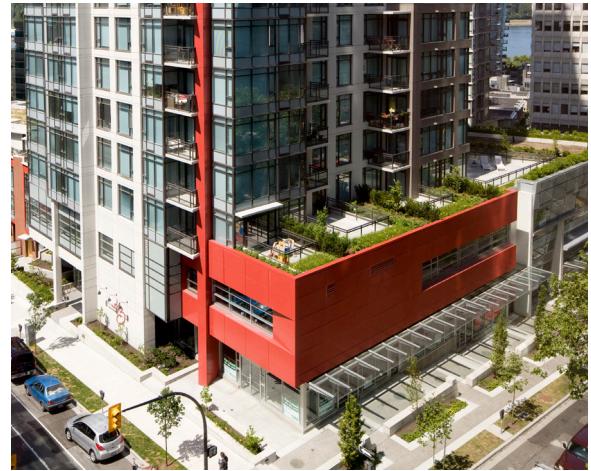


Mechanical equipment screening

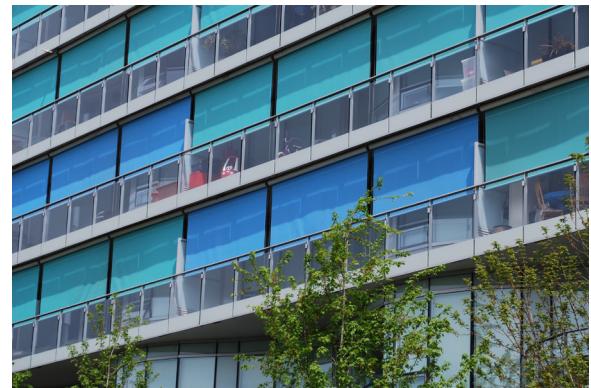
3.6.8 ARCHITECTURAL EXPRESSION

Intent:

Materials and colours are selected by Architects as part of building design and, along with the architectural massing and materiality of the façades, are the most visible aspects that contribute to the character and quality of the public realm. These guidelines are meant to encourage Architects and Engineers to aspire to and execute designs that achieve excellence in both performance and aesthetics.



Use of colour to define corner element



Use of colourful sunshades enliven a neutral façade



Change in material pattern to emphasize building elements, in this case windows and entrances.



Variety of materials to define different façades.

Guidelines:

- a. The use of robust and durable materials are fundamental to sustainable building practices. Materials and systems of high quality that are responsive to the local climate and context are essential. Robust materials used in authentic ways are encouraged, as is the use of materials and methods that have renewable and recycled sources.
- b. The design and choice of colours should consider the quality of light in our region. (ie. colours developed for an intensely sunny low latitude climate are not appropriate). Natural and locally inspired tones are preferred for buildings and streetscapes, and should come from predominately integrally coloured materials.
- c. Materials, colours and patterns should be selected by the Architect through a rigorous design process including an articulated rationale.
- d. Particular attention should be paid to three dimensional composition and appearance of all façades of the building relative to adjacent streets.
- e. Materials selected for buildings and the public realm should be complimentary.
- f. Explore the use of bird friendly glazing strategies such as etched glass, fritted glass, films, decals or other methods to reduce collision risk.

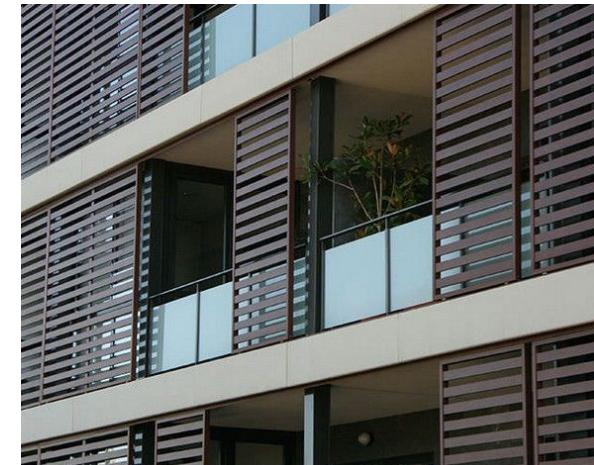


Coordinated colour scheme to break up the façade



Use of durable, solid materials such as brick, metal, and concrete.

- g. Building design should reflect the architectural practices of the time. Recreation and imitation of historic architectural styles are not encouraged.
- h. Buildings at intersections and key focal points should be given special attention to reinforce their role as urban landmarks or gateways. Differentiation in massing, vertical articulation, materials, glazing and other facade enhancing elements should be considered.
- i. Endeavour to design buildings with the lowest possible embodied energy consumption and operational contribution to green house gas emissions.



Sun shading screens integrated into façade



Durable materials – locally sourced, where possible



Contrasting materials to highlight features



Sustainable components such as green walls

draft

4 | Landscape Architecture

In the context of this proposal and as part of this Urban Design Manual, this section is intended to be the most prescriptive. Due to its fundamental importance to the overall development, the illustrations and descriptions are more directives than guidelines. The proposed plaza will be the subject of some interest and importance to the overall development proposal. As such, its size, features and accessibility will be the result of a future detailed design developed with further public input and reviewed and approved by the City.



900-block Landscape Plan

4.1 YATES STREET PLAZA

Design Intent: To create a vibrant plaza on Yates Street that complements the active uses on the street and animates the site with a variety of programming options.

Guidelines:

- a. Situate an active, programmable pedestrian plaza mid block on Yates Street between the two 900-block buildings over structural slab.
- b. Line the plaza with active, grade oriented uses such as retail, restaurants, and lobbies, to promote a safe and animated public space. The plaza provides spill out spaces for the active uses at the edges.
- c. Facilitate the use of the space as a gateway to the site.
- d. Provide a universally accessible route from Yates Street down to View Street open to the public at all times.
- e. Design for flexibility to accommodate a variety of public events and programming.
- f. Incorporate informal play elements that are safe for users of all ages to engage with.
- g. Provide a combination of fixed and moveable seating in the plaza that may include benches, seat steps, seating platforms and off the shelf moveable bistro tables and chairs.
- h. Maximize stormwater capacity using a series of rain gardens and a double row of street trees on Yates Street. Specimen trees are located in raised planters in the plaza. Planters contain native and adapted west coast plant species.
- i. Feature high quality paving throughout the site and specialty paving in a distinct paving pattern in the plaza.
- j. Lighting options may include: plaza lighting (i.e. catenary lighting, bollards, on the ground plane), pedestrian scale lighting and soffit lighting at the building frontage.
- k. Provide weather protection in the form of canopies and awning that promotes the usage of the plaza and patios along Yates Street throughout the seasons.



Yates Street Plaza plan



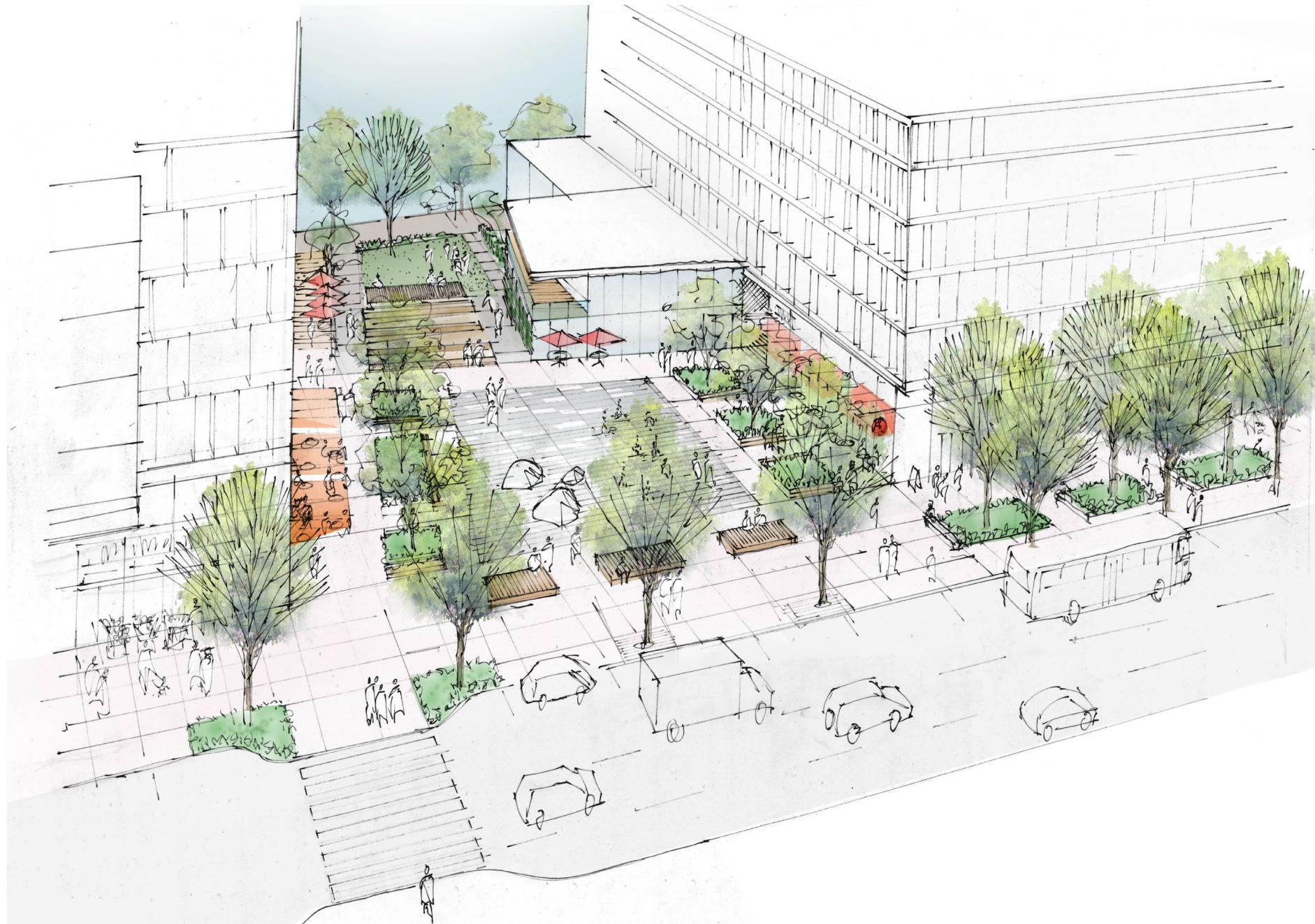
Plaza with informal play elements - Foot of Lonsdale Plaza



Plaza with combination of seating options - Vancouver Art Gallery



Pedestrianization - New Road, UK



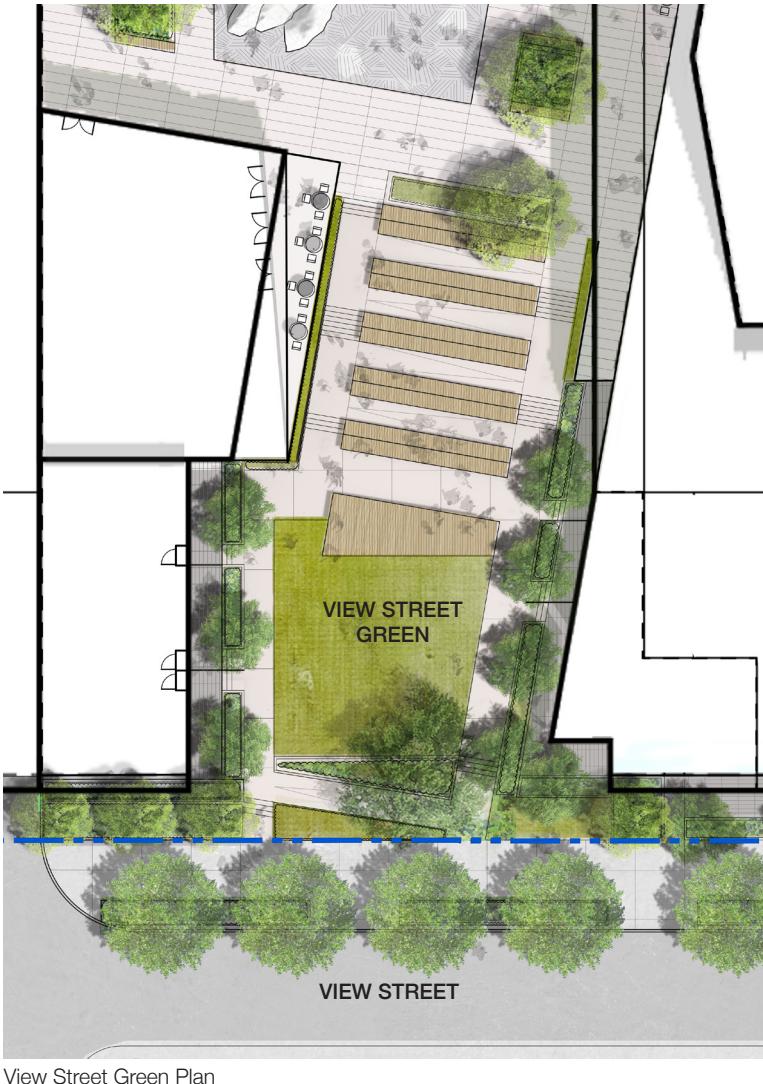
Yates Street Plaza looking southwest

4.2 VIEW STREET GREEN

Design Intent: To transition from the active uses on Yates Street to a green space and less active residential uses on View Street.

Guidelines:

- a. View Street Green is located south of the Yates Street Plaza and Harris Green Terrace between the 900 block townhouses over structural slab. The lawn area flanks the south end of the terrace.
- b. Edge the green with grade-related uses such as entrances and townhouse patios to promote a safe and animated public space.
- c. Facilitate the use of the space as a gateway to the site.
- d. Provide a universally accessible route from View Street up to the Yates Street plaza open to the public at reasonable hours.
- e. Accommodate the grade change from Yates to View Street and provide both programmable areas and spaces for informal gathering and quiet contemplation.
- f. Provide a combination of stairs, accessible ramps and seat steps to accommodate the grade change between Yates and View Streets.
- g. Feature fixed seating in the View Street Green that may include benches and seating platforms.
- h. Plant specimen trees in raised planters on the Green and street trees in rain gardens or tree pits.
- i. Plant areas with low shrubs, groundcovers and perennials in a native and adapted west coast plant palette.



Seat steps and ramps - The Highline, New York



Green space - Pancras Square, London



Variety of seating - Pancras Square, London



From Yates Street Plaza looking south

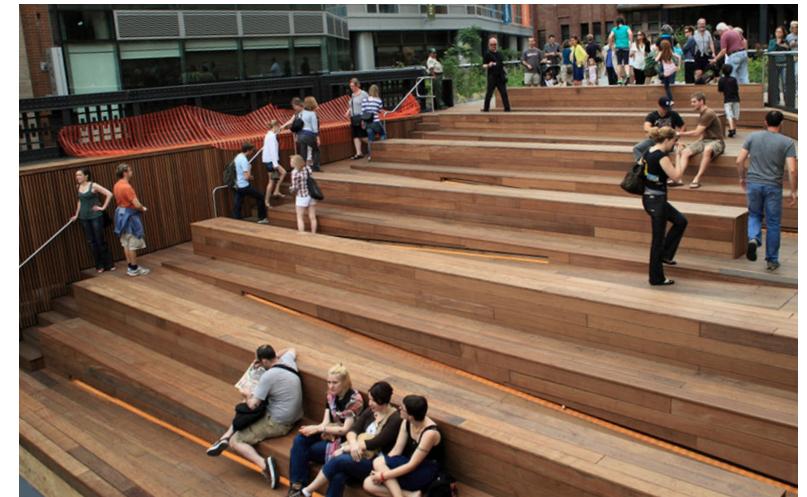
4.3 HARRIS GREEN TERRACE

Design Intent:

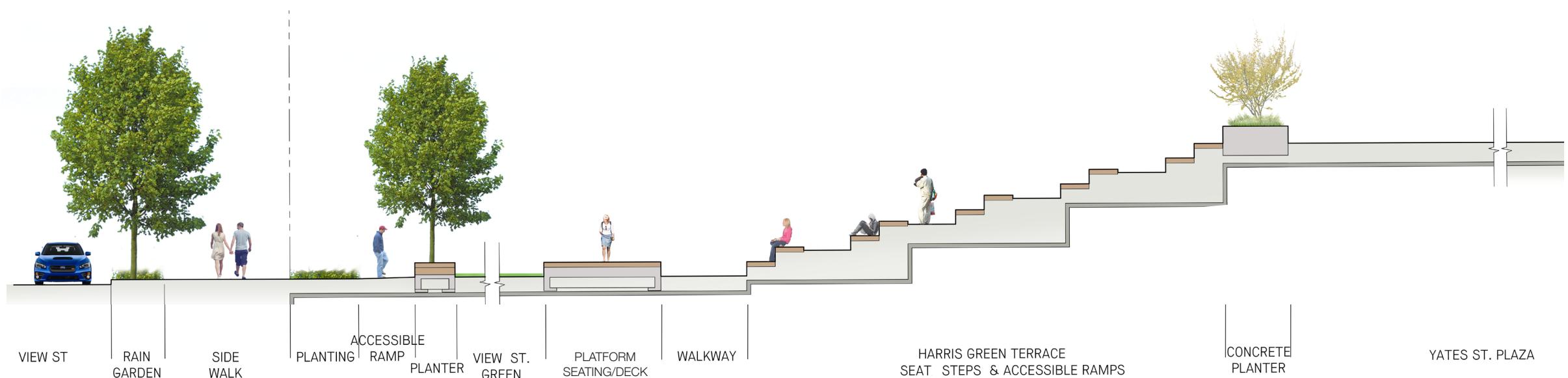
Provide accessible access through the site connecting Yates to View Street, amphitheatre seating and programmable space for performances and special events, every day spaces for informal gathering and quiet contemplation. The Terrace overlooks the lawn at View Street Green.

Design Strategies:

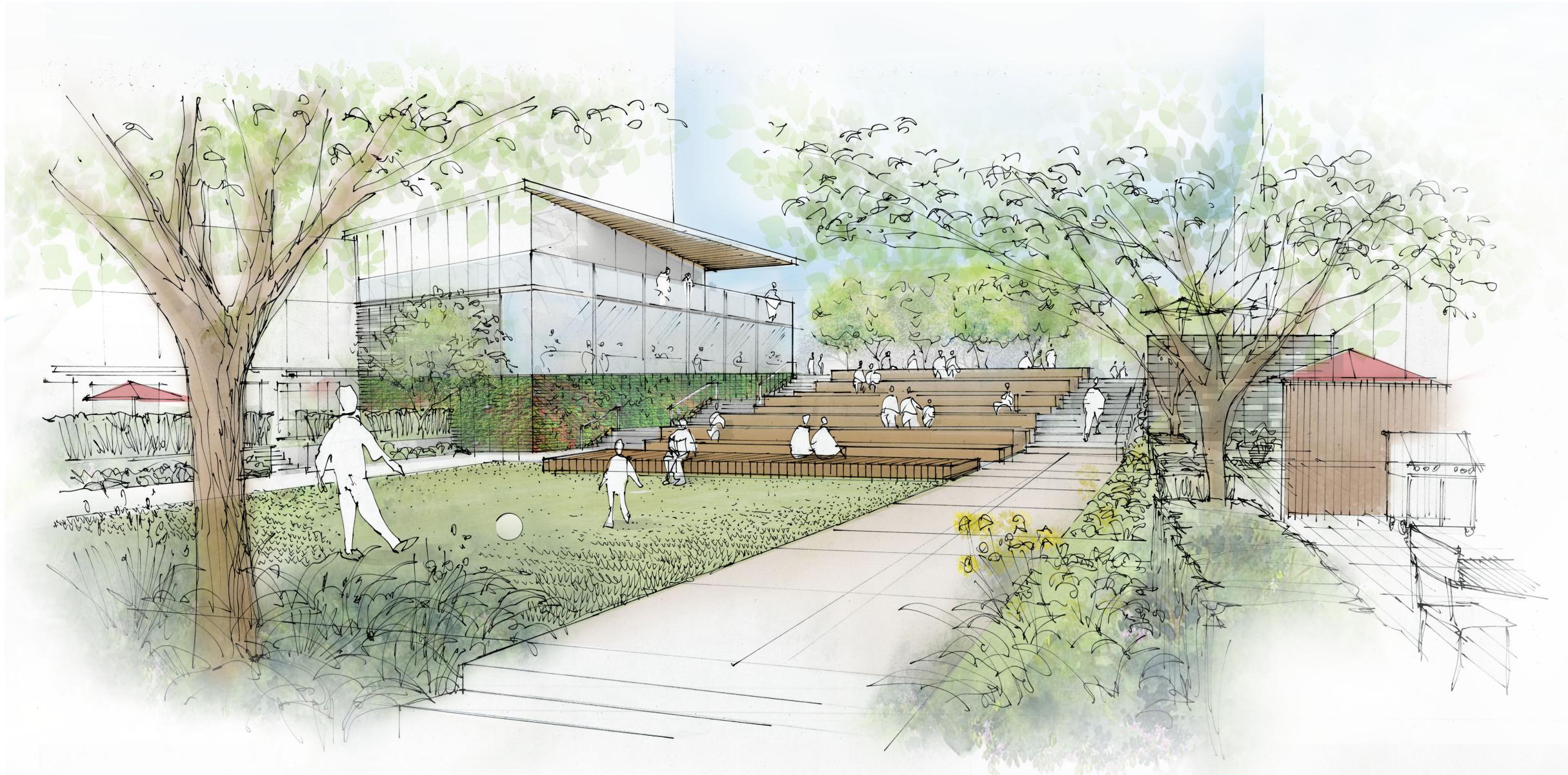
- Provide a combination of stairs, accessible ramps ≤ 5 percent and seat steps.
- Provide a platform deck for both seating and a performance stage area for special events.
- Feature planting in a raised concrete planter that frames the terrace steps.



Terraced seat steps with accessibility ramp incorporated



Harris Green Terrace and View Street Green Section



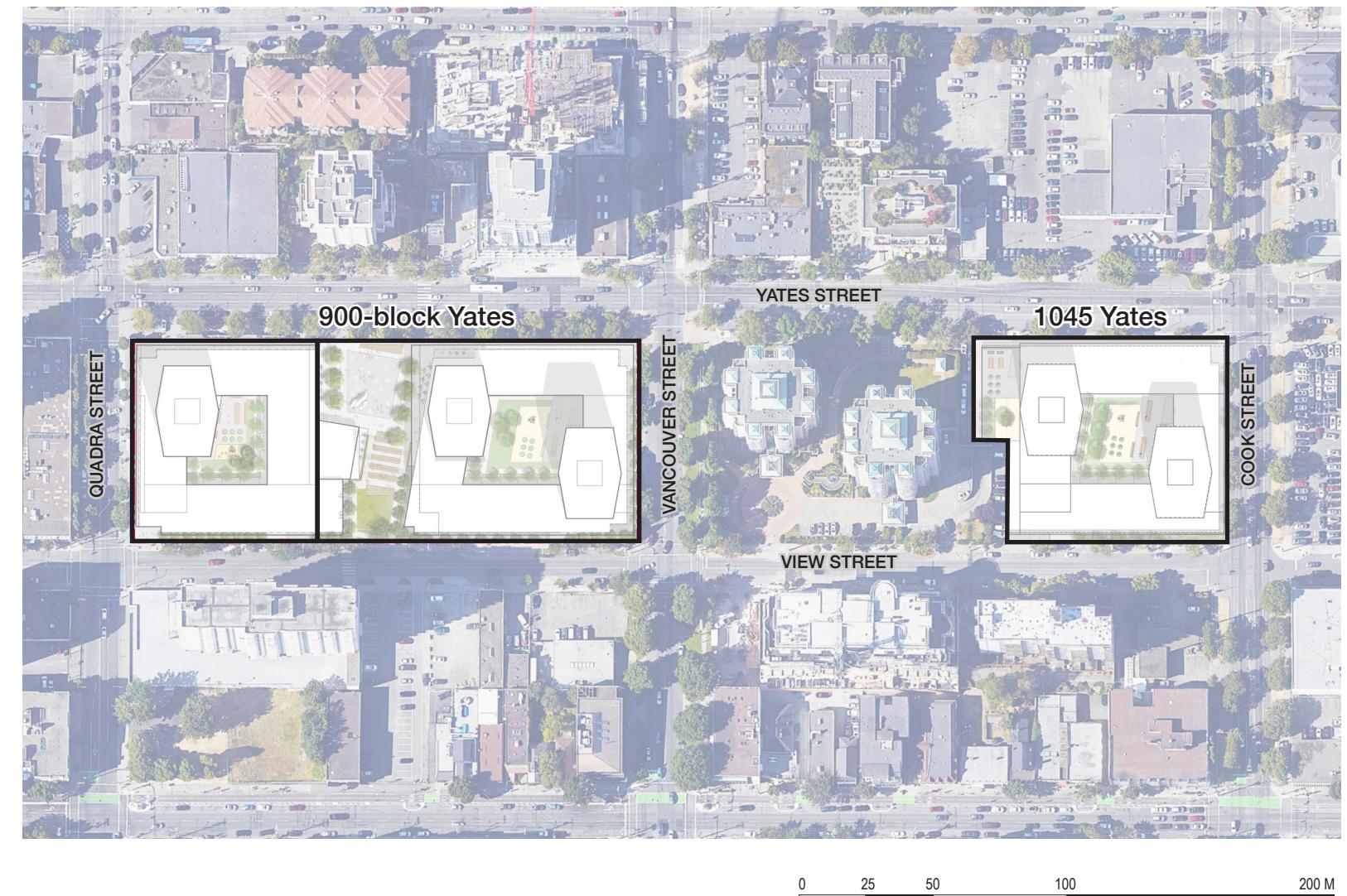
From View Street looking northwest toward View Street Green and Harris Green Terrace

draft

5 | Phasing

The two sites will be constructed in three phases, each self-sufficient in relation to its parking needs.

The sequence of phasing will be determined in the future as a function of market demands.



draft

6 | Definitions

Façade

The face of a building, especially the portion that looks onto a street or open space.

Human scale

Of a size and shape that is relatable to an average person. In an urban design context, the street and building frontages should feel and look good to someone standing at street level, rather than hovering in the sky.

Sky view

Sky view is the amount of sky seen from a street, park, or other open space above and in between building masses. Loss of sky view reduces access to light, which affects the comfort, quality, and use of the public realm. (adapted from Toronto TBDG)

Street wall

The street facing façade of a building or series of buildings that define the edge of the public realm.

Tall buildings

Used to refer to buildings that are located above the street wall/podium and are limited in floor plate size.

Pedestrian

A person who is walking. In the context of this document pedestrian is intended to include persons with strollers, mobility supports such as wheelchairs, walkers and scooters.

Public realm

Publicly accessible exterior space in the form of streets, plazas, terraces and green spaces.



Human scale



Sky view



Street wall



Pedestrian



Public realm